

DEVELOPMENT PATTERN ANALYSIS

Unified Development Code Update

April, 2024



city of

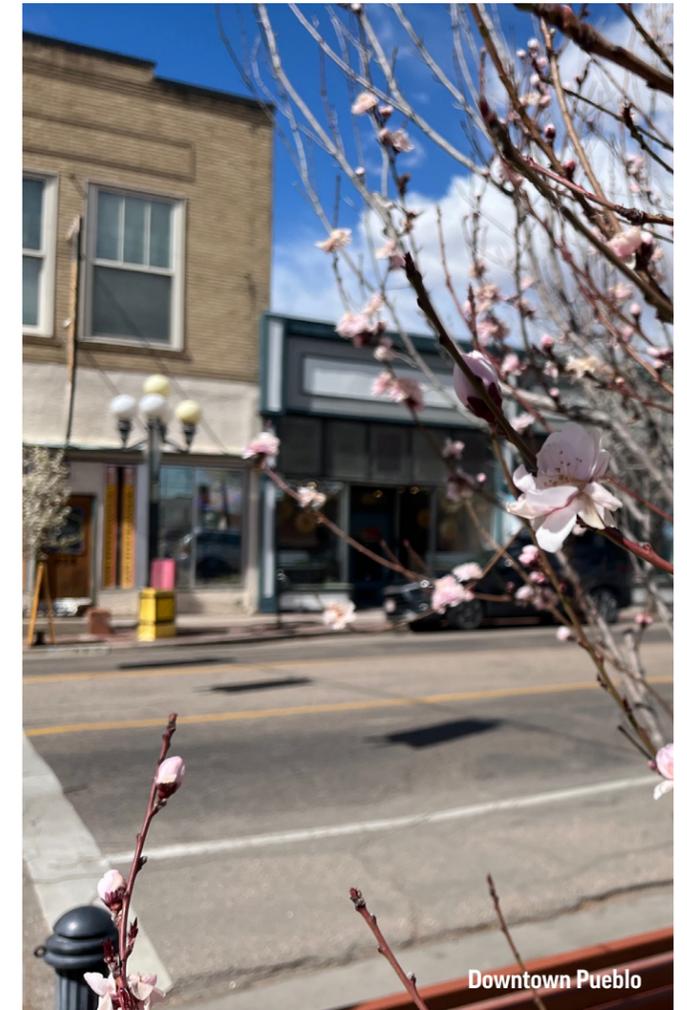
PUEBLO

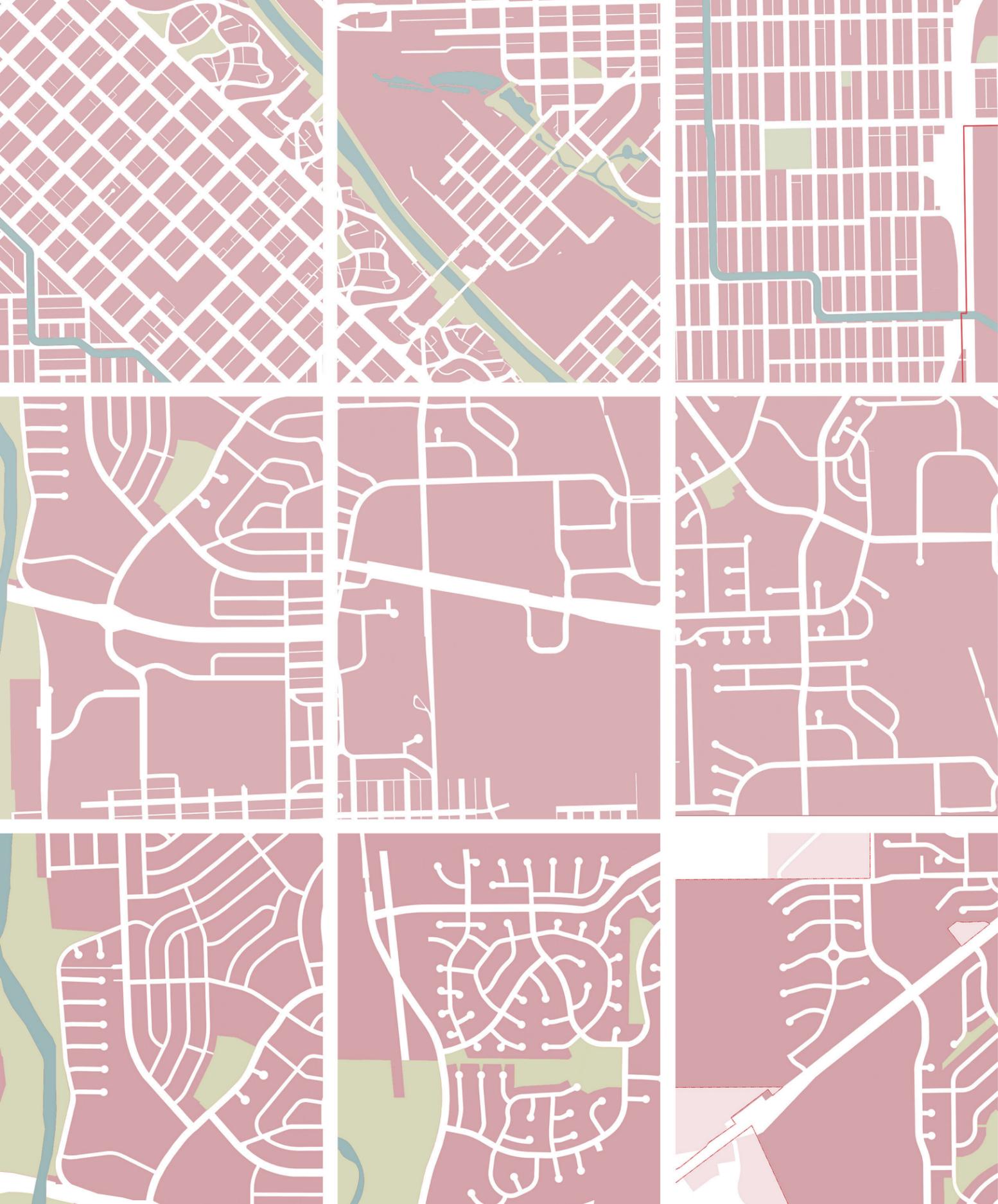
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Introduction

What is a Development Pattern Analysis?

The Development Pattern Analysis provides a visual and diagrammatic analysis of common physical development patterns found throughout the City of Pueblo. The goal of this document is to begin exploring the impact that different types of development patterns can have on the city, and to provide a starting point for community conversations about the Unified Development Code Update.

This document explores four typical development patterns found throughout Pueblo: Grid, Transition, Suburb, and Edge. The Grid, Transition, and Suburb development patterns include both residential and non-residential contexts, while the Edge development pattern only includes a residential context. These four patterns are then broken down into their most typical physical elements: block pattern and size, lot size, open space types, frontage elements, typical street elements, typical building types and frontage elements, and presence or lack of standard urban design features. After documenting each of these elements, a "Key Takeaways" section will explore their impact. Together with the Code Assessment and the Best Practices documents, this Development Pattern Analysis will provide parameters against which to assess the current code and opportunities for improvement.

Development patterns directly impact how the people of Pueblo experience their communities, live in their neighborhoods, and move through the city. As the Pueblo community begins the process of defining the types of places they want to encourage and reinforce through the Unified Development Code update, this analysis will help stakeholders think about the design elements that are necessary to create and nurture those places.

Guiding Principles

Guiding Principles are a useful tool for ensuring that a Unified Development Code update remains aligned with a community's goals, as stated in the Comprehensive Plan. The Pueblo Regional Comprehensive Plan includes many goals that are relevant to the Unified Development Code update, and which can be categorized into the following four Guiding Principles:



Housing Options & Neighborhood Design



Strong Local & Regional Connections



Grow Smarter: Productive Districts, Corridors, & Centers



Celebrate Pueblo: Emphasize Distinct Community Character

As you navigate the document, you will see these icons at the end of each chapter within the "Key Takeaways" page. Each takeaway will be correlated with the relevant Guiding Principle(s), showing how different physical elements of development relate back to the goals of the Pueblo Regional Comprehensive Plan.

Historic Development Patterns in Pueblo

Early maps and historic imagery of Pueblo from the early 20th century shows the city developing according to the era's highly connected, walkable, and people-oriented urban design and development practices. Wide, boulevard-style roads formed an interconnected grid. Commercial and mixed-use buildings of many types were built to the edge of the public right-of-way, helping to maintain pedestrian-oriented frontage patterns along the streets. Even as early suburbs like the Belmont and Sunset neighborhoods began to deviate from the traditional grid pattern, they still maintained a highly interconnected street network. Community destinations, like schools, often formed the "centers" of these neighborhoods.



Downtown Pueblo, 1924

Source: The Digital Collection at Pueblo City-County Library District

The structure and legacy of these historic development patterns can still be seen throughout many of the Grid-pattern areas of Pueblo today. The gridded street and alley network in Pueblo is remarkably well-preserved, many historic and older buildings remain standing, and historic mixed-use and commercial corridors throughout neighborhoods like Mesa Junction, Bessemer, and Northside still exist with many thriving businesses serving nearby neighborhoods.



Downtown Pueblo, 1934

Source: The Digital Collection at Pueblo City-County Library District



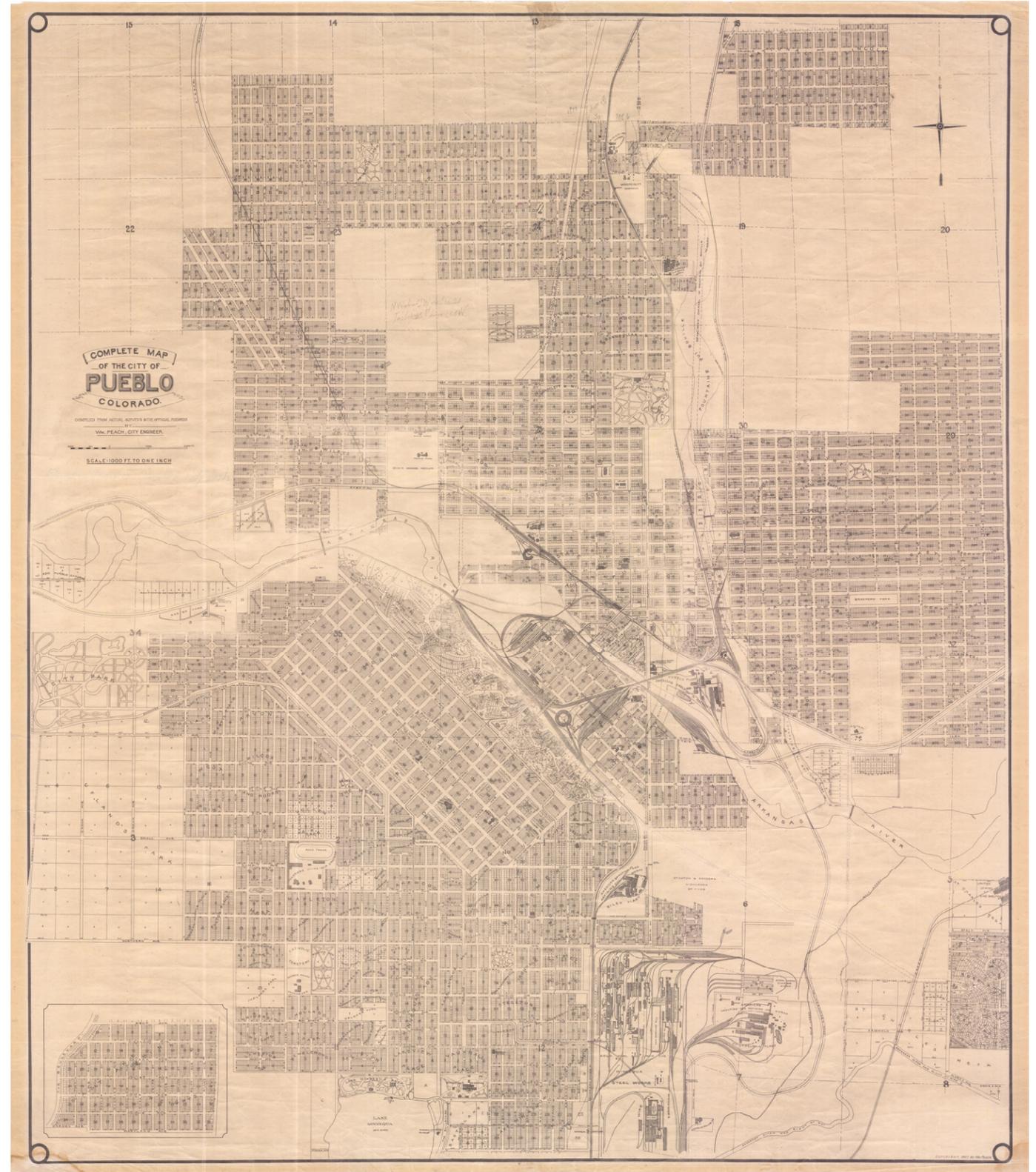
Mesa Junction Neighborhood, 1950

Source: The Digital Collection at Pueblo City-County Library District



Belmont Neighborhood, unknown year

Source: The Digital Collection at Pueblo City-County Library District



City of Pueblo, 1907.

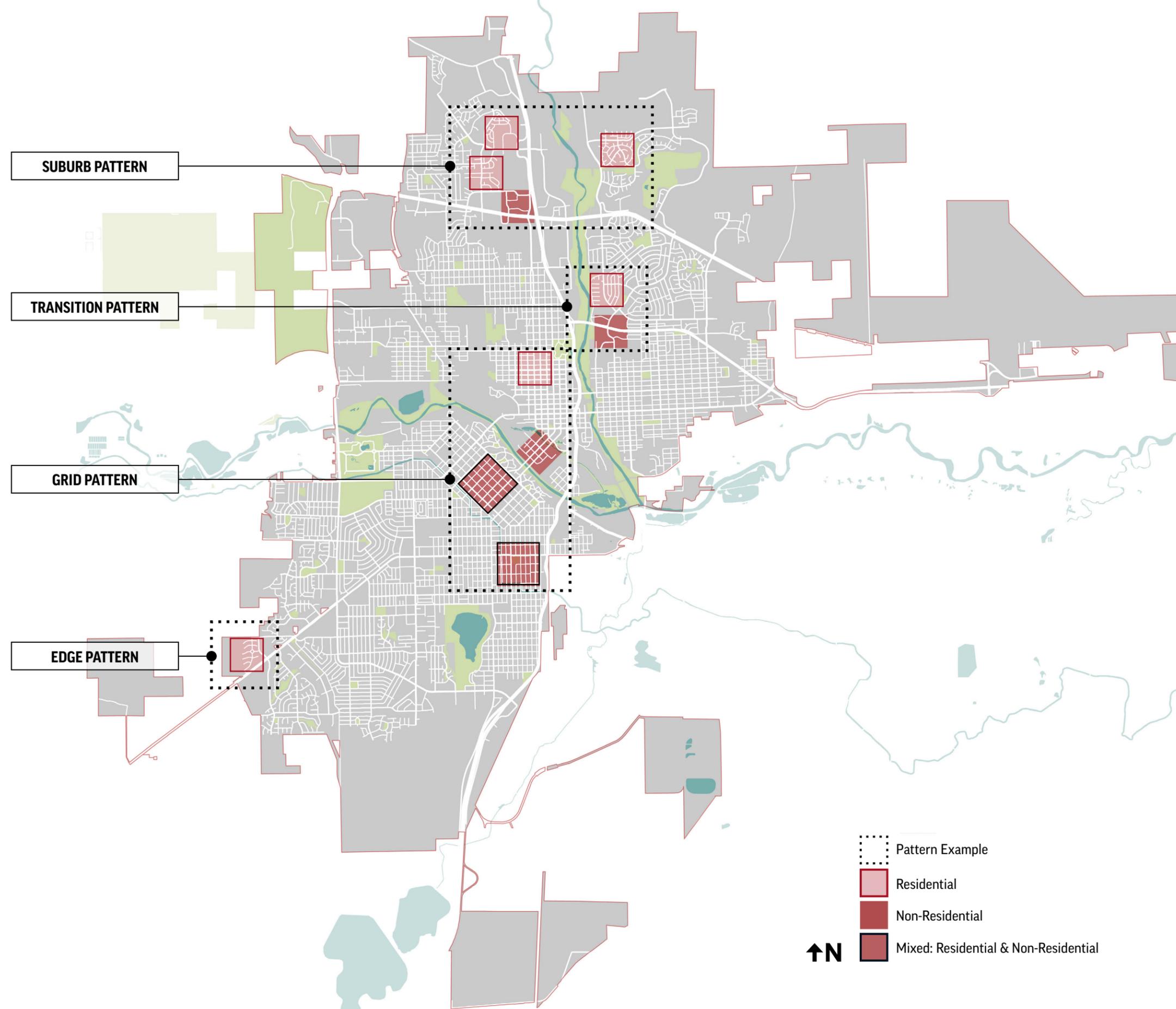
Source: HistoricPueblo.org. [<https://www.historicpueblo.org/images/other-maps/1907Pueblo.pdf>]

Pueblo Today

Development Pattern Map

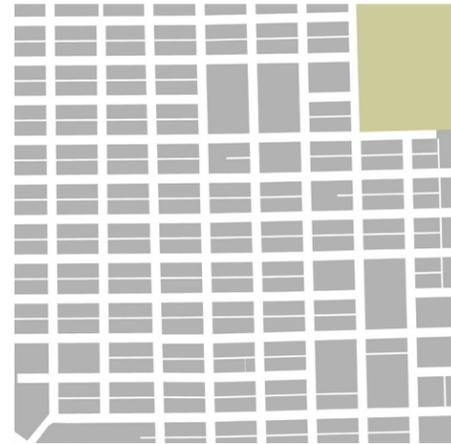
Today, Pueblo contains a variety of different patterns of development that create different kinds of places within the city. The map to the right illustrates the location of each type of development pattern analyzed in this document. The areas outlined are not the only instances of that particular type of development pattern within Pueblo; rather, they have been selected and analyzed as *typical* examples of each development pattern.

Four typical development patterns are identified: Grid, Transition, Suburb, and Edge. The Grid, Transition, and Suburb patterns include residential and non-residential contexts, while the Edge pattern only includes a residential context.



Development Pattern Overview: Residential

GRID



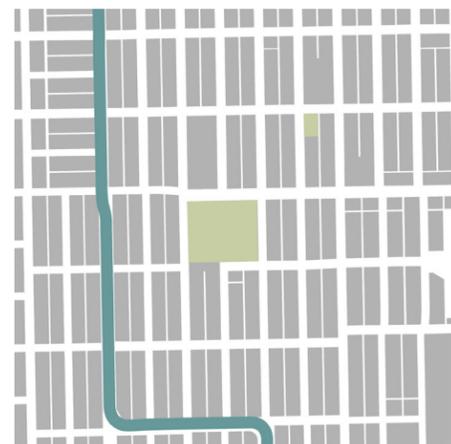
Neighborhood
Northside

Block Size
Approx. 300' by 400',
2.8 acres

Open Space Types
Regional Park,
Neighborhood Park,
Specialized Facility, &
Open Space

Land Use
Residential, Open Space,
Commercial

Historic Downtown Residential District



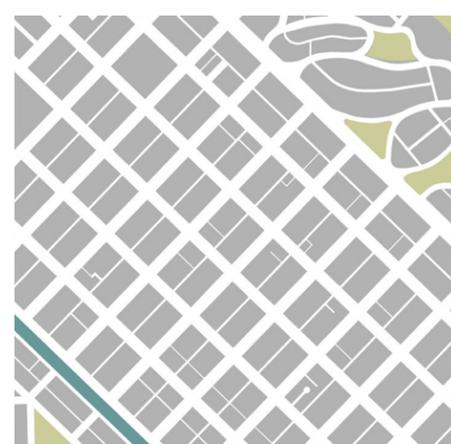
Neighborhood
Bessemer

Block Size
Approx. 294' by 620' -
287' by 577',
2.8 acres - 4.2 acres

Open Space Types
Neighborhood Park,
School Park, & Open
Space

Land Use
Residential, Commercial,
Institutional, Mixed-Use,
Open Space

Historic Traditional Mixed-Use District



Neighborhood
Mesa Junction

Block Size
Approx. 452' by 452',
4.7 acres

Open Space Types
Neighborhood Park,
Neighborhood Square, &
Open Space

Land Use
Residential, Commercial,
Institutional, Mixed-Use,
Open Space

Historic Square Mixed-Use District

TRANSITION



Neighborhood
Belmont

Block Size
Varies, approx. 600' by
200' - 1,840' by 800',
2.6 acres - 25 acres

Open Space Type
Regional Park, School
Park, & Open Space (trail
corridor)

Land Use
Residential, Open Space

Mixed Housing Neighborhood

SUBURB



Neighborhood
University

Block Size
Varies, approx. 306' by
370' - 1,320' by 572',
2.7 acres - 14 acres

Open Space Type
Neighborhood Park,
Specialized Facility, &
Open Space (trail)

Land Use
Residential, Open Space

Detached Housing Suburb



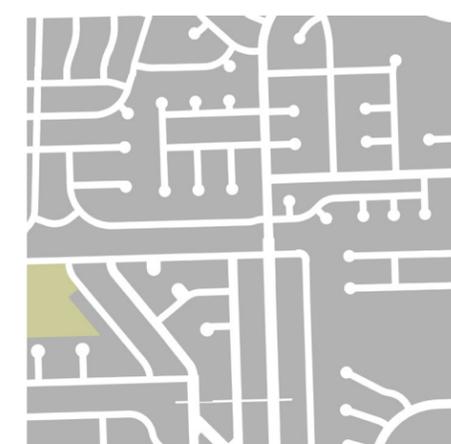
Neighborhood
Ridge

Block Size
Varies, approx. 530' by
200' - 1,320' by 572',
1.8 acres - 31 acres

Open Space Type
School Park & Open
Space

Land Use
Residential

Mixed Housing Suburb



Neighborhood
Ridge

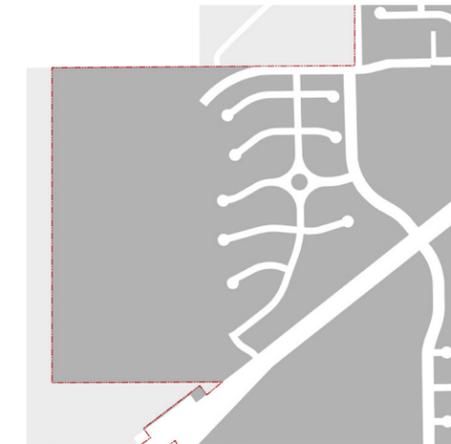
Block Size
Varies, approx. 270' by
490' - 1,100' by 900',
3.2 acres - 11.9 acres

Open Space Type
School Park & Open
Space

Land Use
Residential, Open Space

Mixed Housing Suburb

EDGE



Neighborhood
Regency

Block Size
Varies, approx. 400' by
920' - 540' by 1,500',
8 acres - 20 acres

Open Space Type
Neighborhood Park &
School Park

Land Use
Residential

Edge Neighborhood

0 250 500 1000 Feet

Development Pattern Overview: Non-Residential

GRID



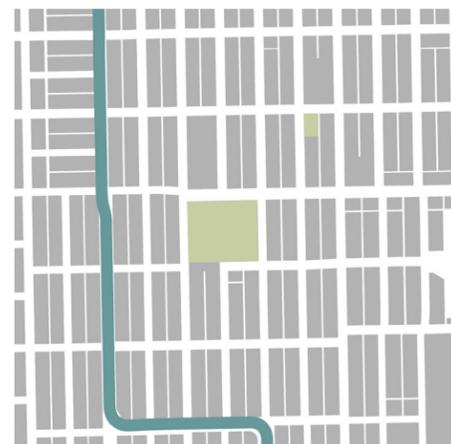
Neighborhood
Downtown

Block Size
Approx. 338' by 434',
3.4 acres

Open Space Type
Regional Park,
Neighborhood Park,
Open Space, & Natural

Land Use
Residential, Commercial,
Institutional, Mixed-Use,
Open Space

Historic Downtown Business District



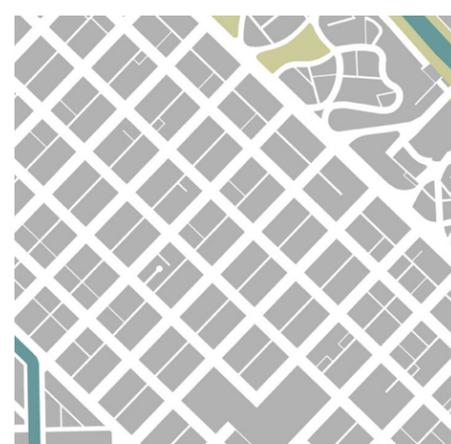
Neighborhood
Bessemer

Block Size
Approx. 294' by 620'
through 287' by 577',
2.8 acres - 4.2 acres

Open Space Type
Neighborhood Park,
School Park, & Natural

Land Use
Residential, Commercial,
Institutional, Mixed-Use,
Open Space

Historic Traditional Mixed-Use District



Neighborhood
Mesa Junction

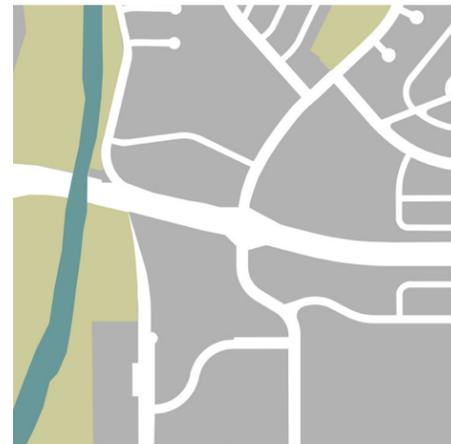
Block Size
Approx. 452' by 452',
4.7 acres

Open Space Type
Neighborhood Park &
Natural

Land Use
Residential, Commercial,
Institutional, Mixed-Use,
Open Space

Historic Square Mixed-Use District

TRANSITION



Neighborhood
Belmont & East Side

Block Size
Varies, approx. 530' x
1,155' - 1,340' x 2,200',
11 acres - 30 acres

Open Space Type
Regional Park, School
Park, & Open Space (trail
corridor)

Land Use
Commercial, Office,
Hospitality, Institutional,
Open Space

Commercial Center

SUBURB



Neighborhood
Ridge & North Side

Block Size
Varies, approx. 510' by
420' - 1,660' by 1,400',
5 acres - 52 acres

Open Space Type
N/A

Land Use
Commercial, Office,
Institutional, Hospitality

Commercial District

EDGE

N/A

0 250 500 1000 Feet



Downtown Pueblo

1 GRID

Introduction

In Pueblo, the Grid development pattern is extensive and well-preserved throughout many neighborhoods. Most commonly found throughout the historic Downtown and in several other historic neighborhoods, the Grid pattern supports highly connected neighborhoods in close proximity to (or integrated within) commercial or mixed-use districts. The City of Pueblo has three distinctive variations of the Grid pattern:

- East-west oriented small rectangular blocks.
- North-south oriented long, narrow rectangular blocks.
- Square blocks offset 45 degrees off from north-south a north-south axis.

The grid pattern allows for the development of walkable neighborhoods with an integrated mix of uses, services, open spaces, and amenities. Therefore, the residential and non-residential contexts often have common characteristics, such as

a continuous sidewalk network, similar or identical block sizes, and alley access.

In residential areas, streets typically have a wide streetscape amenity zone. Although alleys help to reduce the front driveway access (and ensuing interruptions to the sidewalk network), there are some private front driveways and front parking. Noticeably, the spacious streetscape amenity zone becomes a de-facto "on-street" parking zone in many settings.

Non-residential blocks are typically pedestrian-friendly with a well-designed street and streetscape. Parking is located on-street, or to the rear or side of buildings in order to reduce sidewalk interruptions from curb cuts.

Overall, the relationship between the public realm and private development is harmonious and conducive to a variety of transportation modes.



Residential Context 01.

Northside Neighborhood

Historic Downtown Residential District

--- Development Pattern Example Area

The downtown neighborhoods located on the north of the Pueblo's Downtown were formed by a rectilinear "grid" development pattern. These neighborhoods notably integrate a variety of housing types throughout the walkable block structure.



Block Pattern

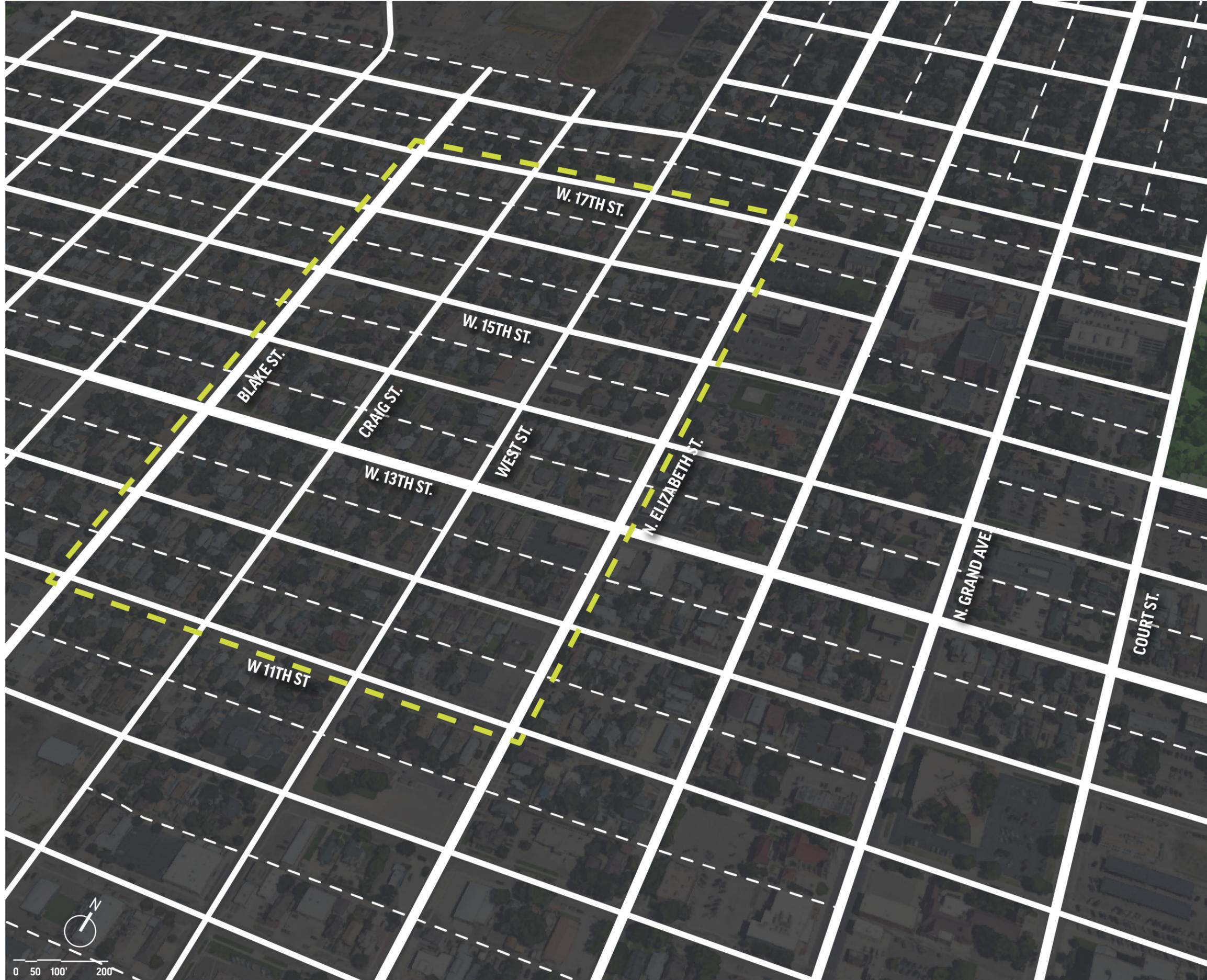
Grid Block Pattern

Block Size: approx. 2.8 acres

Block Dimensions: approx. 300' x 400'

- Street Network
- Trails
- - - Alley Network
- - - Example Boundary
- Parks & Open Space

This grid pattern is characterized by small block sizes, alleys for parking access, a variety of residential housing types, and a connected network of sidewalks. The grid street pattern supports well-connected neighborhoods and commercial areas for active transportation, transit, and vehicular transportation.



Typical Open Space Typologies

The Downtown neighborhoods typically have moderate to large parks. The Mineral Palace Park is a large-scale regional destination park, and the El Pueblo Pride Park is a moderately scaled neighborhood-serving park.

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Mineral Palace Park
Park Type: Regional Park
Size: 46.2 acres



Bike lanes within the park.



El Pueblo Pride Park
Park Type: Neighborhood Park
Size: 5.3 acres

Typical Street Typologies

Downtown neighborhoods have several functional class streets that are typical of the area. The roadway width ranges from 30' to 62'. The majority of streets have a wide streetscape amenity zone, parking on both sides of the streets, and all have a minimum 5.5' wide sidewalk. Overall, Downtown streets are well-connected throughout the neighborhoods.

Note: Roadway widths are measured from curb edge to curb edge.



Sidewalk	Parking	Travel Lane	Travel Lane	Travel Lane	Travel Lane	Parking	Sidewalk
5.5'	8'	12.5'	11'	10.5'	12'	8'	5.5'
Roadway							
62'							

West 13th Street

Functional Class: Major Arterial
 Roadway Width: 62'
 Multimodal Feature(s): N/A



Sidewalk	Buffer	Parking	Travel Lane	Parking	Buffer	Sidewalk
5.5'	16'	7.5'	15'	7.5'	16'	5.5'
Roadway						
30'						

West 11th Street

Functional Class: Collector
 Roadway Width: 30'
 Multimodal Feature(s): Buffered Sidewalks



North Greenwood Street

Functional Class: Minor Arterial
 Roadway Width: 37'
 Multimodal Feature(s): Sharrow, Buffered Sidewalks

Sidewalk	Buffer	Parking	Travel Lane	Sharrow	Parking	Buffer	Sidewalk
6'	11.5'	7.5'	11'	11'	7.5'	10.5'	6'
Roadway							
37'							



Sidewalk	Buffer	Parking	Travel Lane	Parking	Buffer	Sidewalk
6'	16'	7.5'	15'	7.5'	15.5'	6'
Roadway						
30'						

Craig Street

Functional Class: Local
 Roadway Width: 30'
 Multimodal Feature(s): Buffered Sidewalks

Street Design Elements

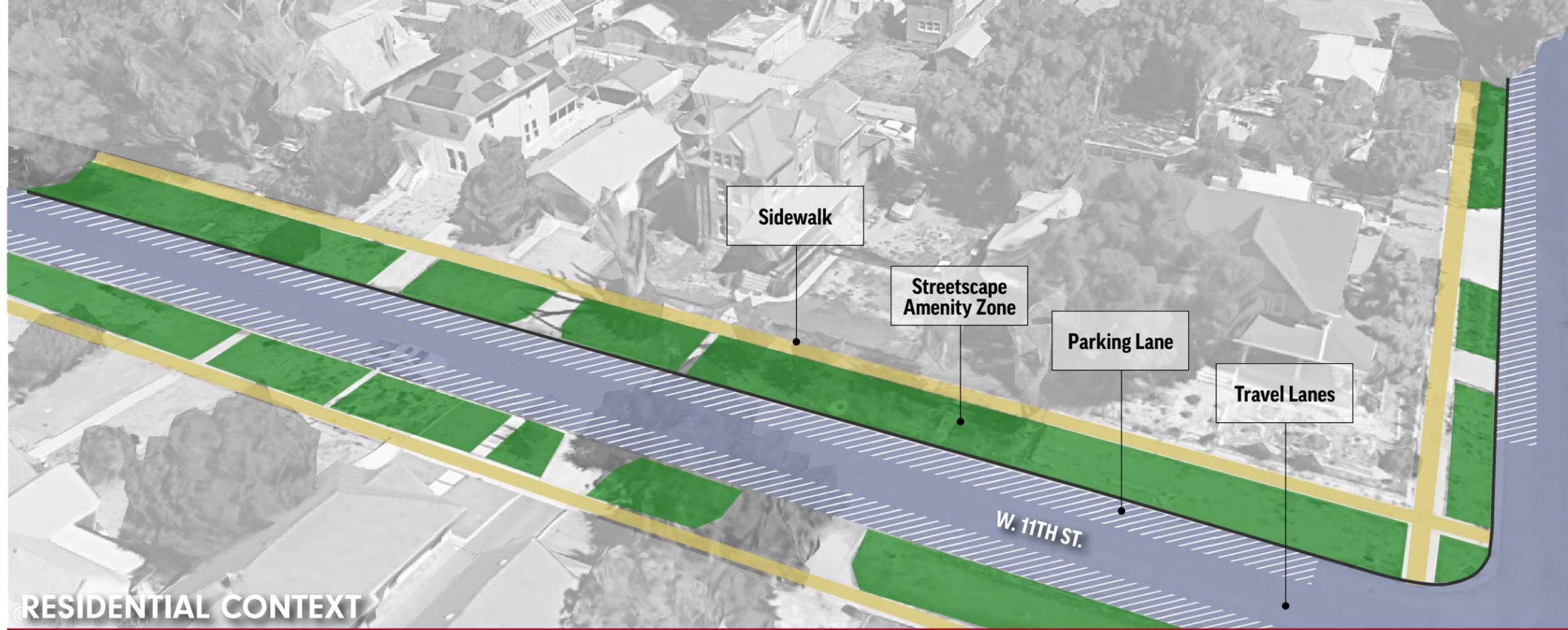
Street Design Elements

- Sidewalk
- Bike Lane
- Streetscape Amenity Zone
- Parking
- Travel Lane

Street Design & Transition

W. 11th St. is one example of a street in this pattern that transitions from a residential context to a non-residential context. In its residential context, it has a wide streetscape amenity zone and on-street parking zones on both sides of the streets. As the street moves into non-residential areas, the street design alters to favor vehicular transportation: the roadway width is increased, the on-street parking zones are discontinued, and only one side of the sidewalk has a streetscape amenity zone.

Context	Residential	Non-residential
Roadway Width	26'	32'
Roadway	On-street parking on both sides of the streets	N/A
Streetscape	Wide landscape buffer on both sides of the streets	Landscape buffer on one side



RESIDENTIAL CONTEXT



NON-RESIDENTIAL CONTEXT

Typical Urban Design Elements

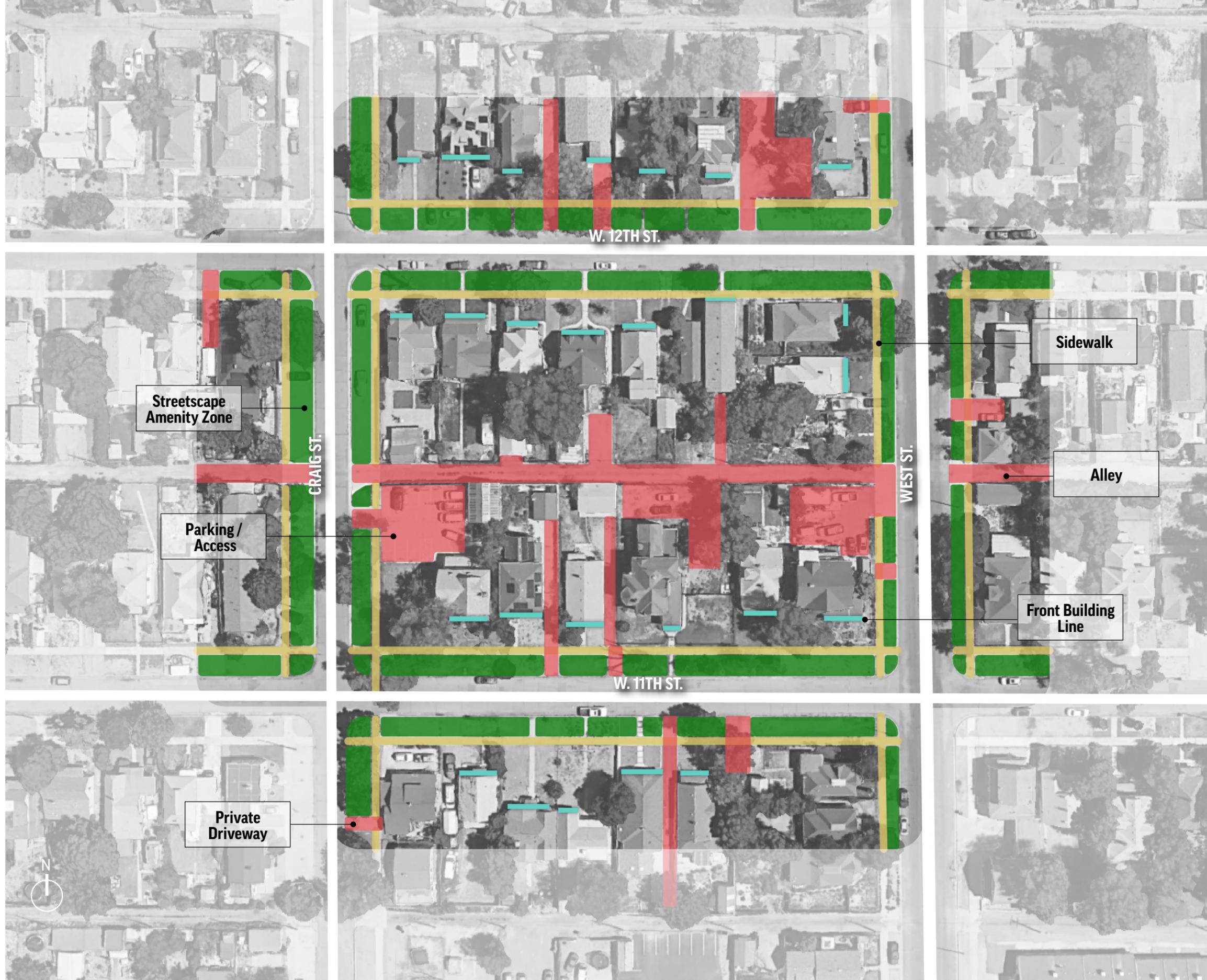
Urban Design Elements

- Front Building Line
- Sidewalk
- Access (Driveways & Parking)
- Streetscape Amenity Zone

Front building lines in Downtown neighborhoods form a consistent pattern. There is good sidewalk presence, although shared driveways or increased alley access could reduce sidewalk interruptions from driveways. Driveways typically lead to rear detached garages, or parking areas located to the rear or side of lots. The streetscape amenity zone buffers sidewalk users from the roadway, but could be improved by increasing street lights, benches, shade, and other amenities that create a safe and comfortable environment for active transportation.



Example Block Location



Typical Urban Design Elements

A series of diagrams shows a closer look at a portion of the example block shown in plan view on the previous page.

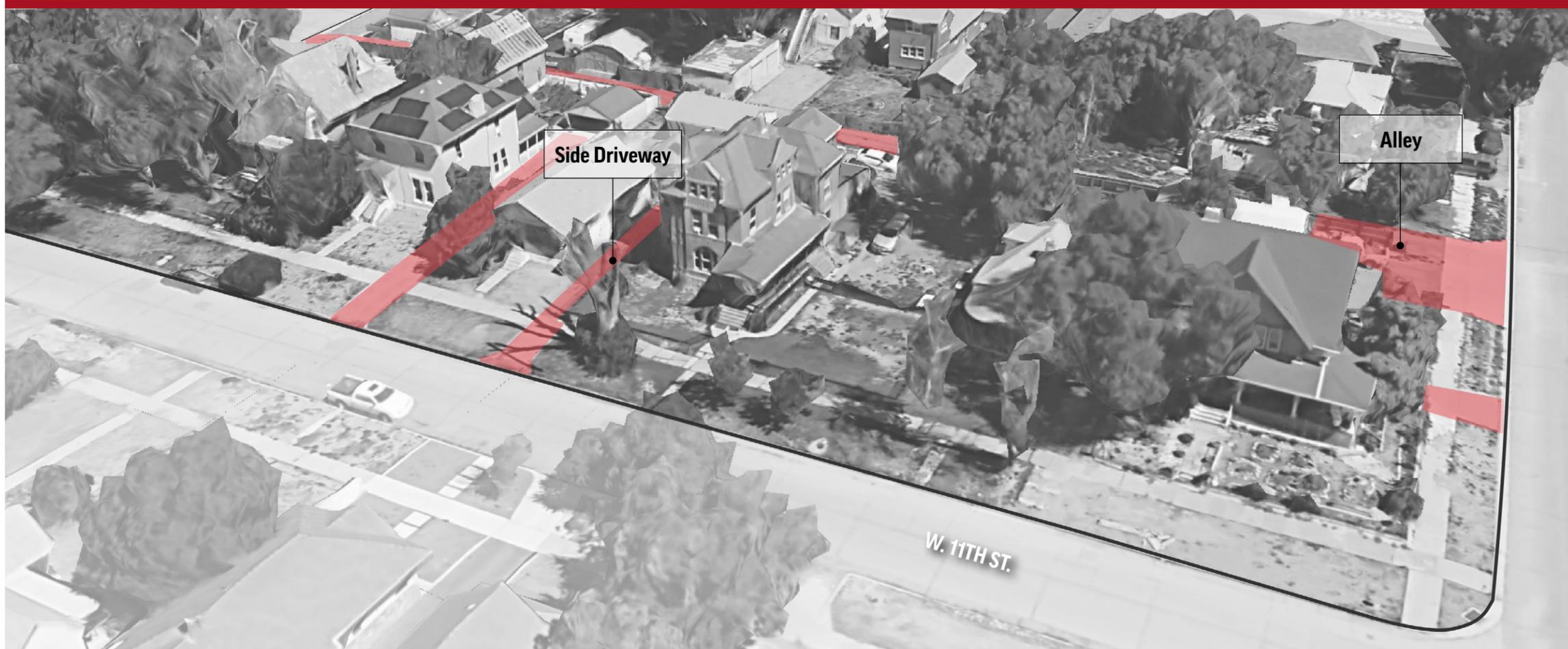
Front Building Line

The example block has a well-established frontage to connect to the sidewalks.



Access (Driveway & Parking)

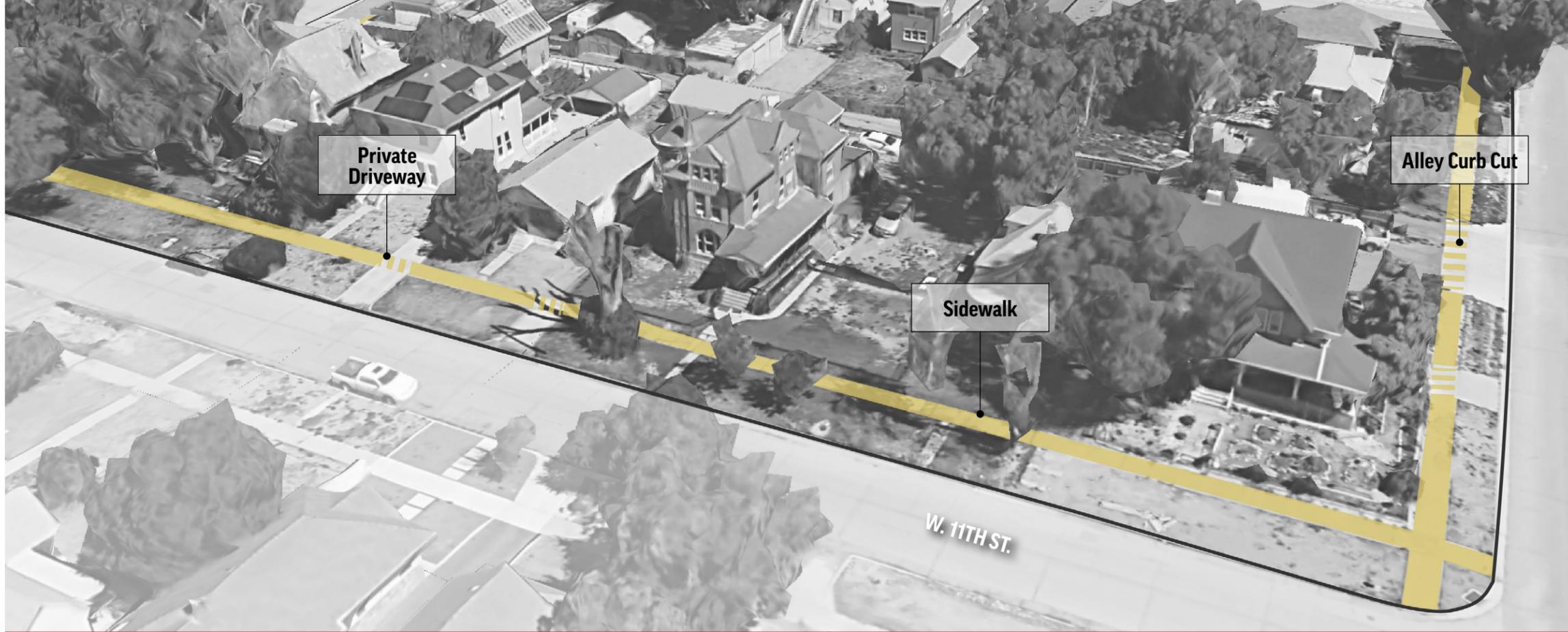
The alley of the block helps to reduce the front driveway access to garage / parking which enhances the pedestrian experience on sidewalks.



Typical Urban Design Elements

Sidewalk

The example block has continuous sidewalks around the whole block but has some interruptions from private driveways.



Streetscape Amenity Zone

The example block has wide streetscape amenity zone, ranging from 7' to 14' wide.



Frontage Types

The downtown neighborhoods have a variety of entry feature types that help to establish the relationship between private development and the public realm. Alleys also help to reduce the front driveway access and improve pedestrian experience and safety on sidewalks.

Frontage & Access

- Entry Features
- Access (Driveways & Parking)



Entry Features: Porch & Stoop | Access Format: Alley & Side Driveways
Parking Format: Rear Open Site



Entry Features: Porch & Portico | Access Format: Alley
Parking Format: Rear Detached Garage



Entry Features: Stoop | Access Format: Alley & Front Driveways
Parking Format: Rear Open Site



Entry Features: Porch, Portico, & Stoop | Access Format: Alley & Front Driveway
Parking Format: Rear Detached Garage



Entry Features: Recessed | Access Format: Alley & Side Driveways
Parking Format: Side Driveways, Rear Open Site, & Detached Garage



Entry Features: Porch | Access Format: Alley & Front Driveway
Parking Format: Rear Attached Garage



Entry Features: Porch, Portico, & Stoop | Access Format: Alley
Parking Format: Rear Detached Garage



Entry Features: Portico & Stoop | Access Format: Alley, Front, & Side Driveway
Parking Format: Rear & Side Parking Lot

Building Types

The Downtown neighborhoods have a wide variety of housing types, from single-family detached houses, duplexes, and multi-unit houses to moderately-scaled apartment buildings integrated into the neighborhoods.



Detached House (Standard Lot)

Scale: Small | Building Footprint: approx. 1,865 ft²
 Lot Size: approx. 5,450 ft² | Height: 2 stories | Frontage Type: Neighborhood Yard



Detached House (Small Lot)

Scale: Small | Building Footprint: approx. 1,630 ft²
 Lot Size: approx. 3,140 ft² | Height: 1 story | Frontage Type: Neighborhood Yard



Multi-unit House (4 Units)

Scale: Small | Building Footprint: approx. 1,650 ft²
 Lot Size: approx. 3,930 ft² | Height: 2 stories | Frontage Type: Neighborhood Yard



Multi-unit House (5 Units)

Scale: Medium | Building Footprint: approx. 1,930 ft²
 Lot Size: approx. 6,030 ft² | Height: 2 stories | Frontage Type: Neighborhood Yard



Detached House (Narrow Lot)

Scale: Small | Building Footprint: approx. 970 ft²
 Lot Size: approx. 2,650 ft² | Height: 1 story | Frontage Type: Neighborhood Yard



Duplex

Scale: Small | Building Footprint: approx. 2,690 ft²
 Lot Size: approx. 5,700 ft² | Height: 2 stories | Frontage Type: Neighborhood Yard



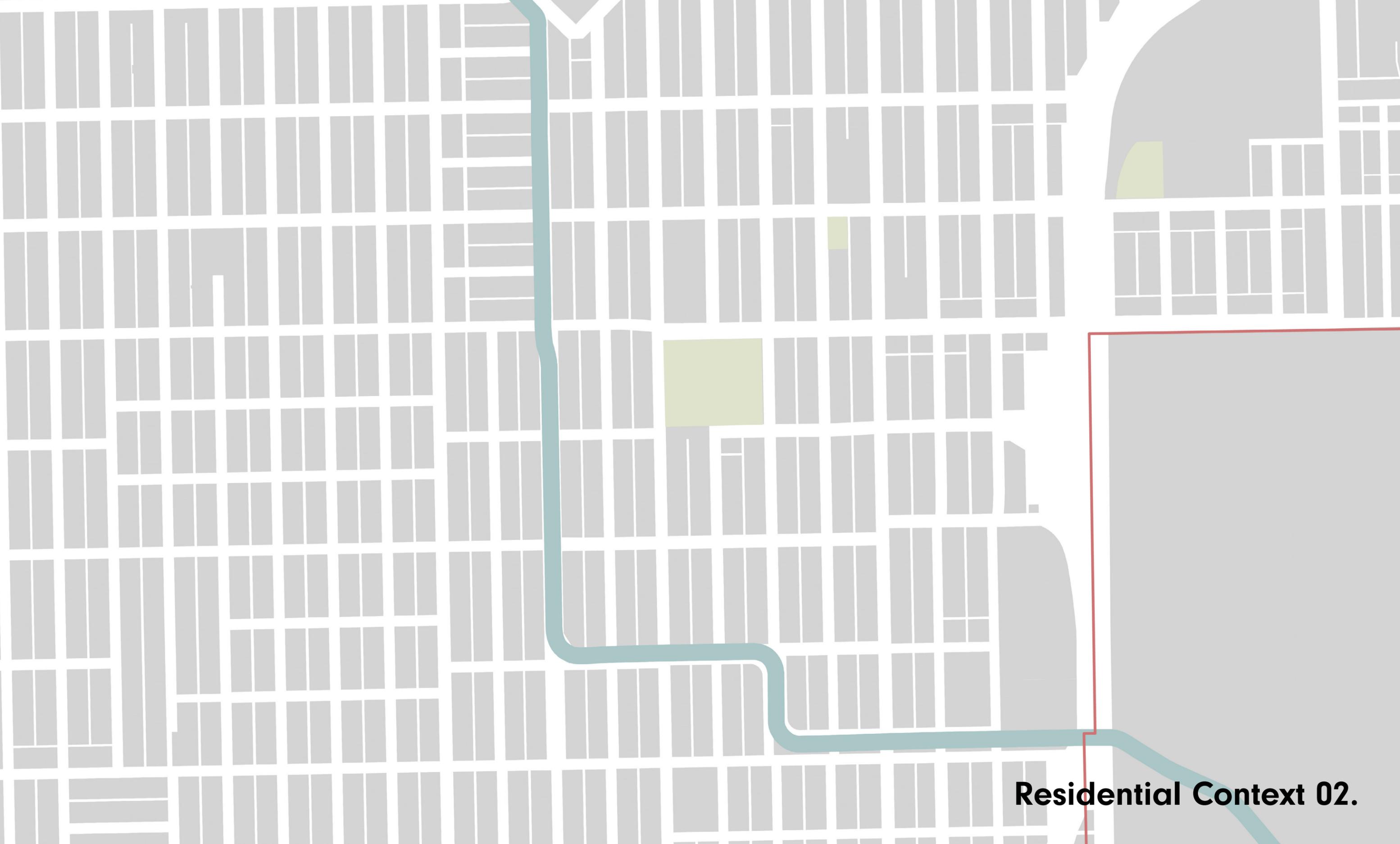
Apartment Building

Scale: Medium | Building Footprint: approx. 5,590 ft²
 Lot Size: approx. 10,560 ft² | Height: 2 stories | Frontage Type: Street-Front



Apartment Building

Scale: Large | Building Footprint: approx. 6,760 ft²
 Lot Size: approx. 21,080 ft² | Height: 2 stories | Frontage Type: Street-Front



Residential Context 02.

Bessemer Neighborhood

Historic Traditional Mixed-Use District

--- Development Pattern Example Area

The Bessemer Neighborhood is a historic neighborhood in Pueblo and is unique for its long, narrow block pattern. It includes many traditional "shotgun" style homes on long and narrow lots.



Block Pattern

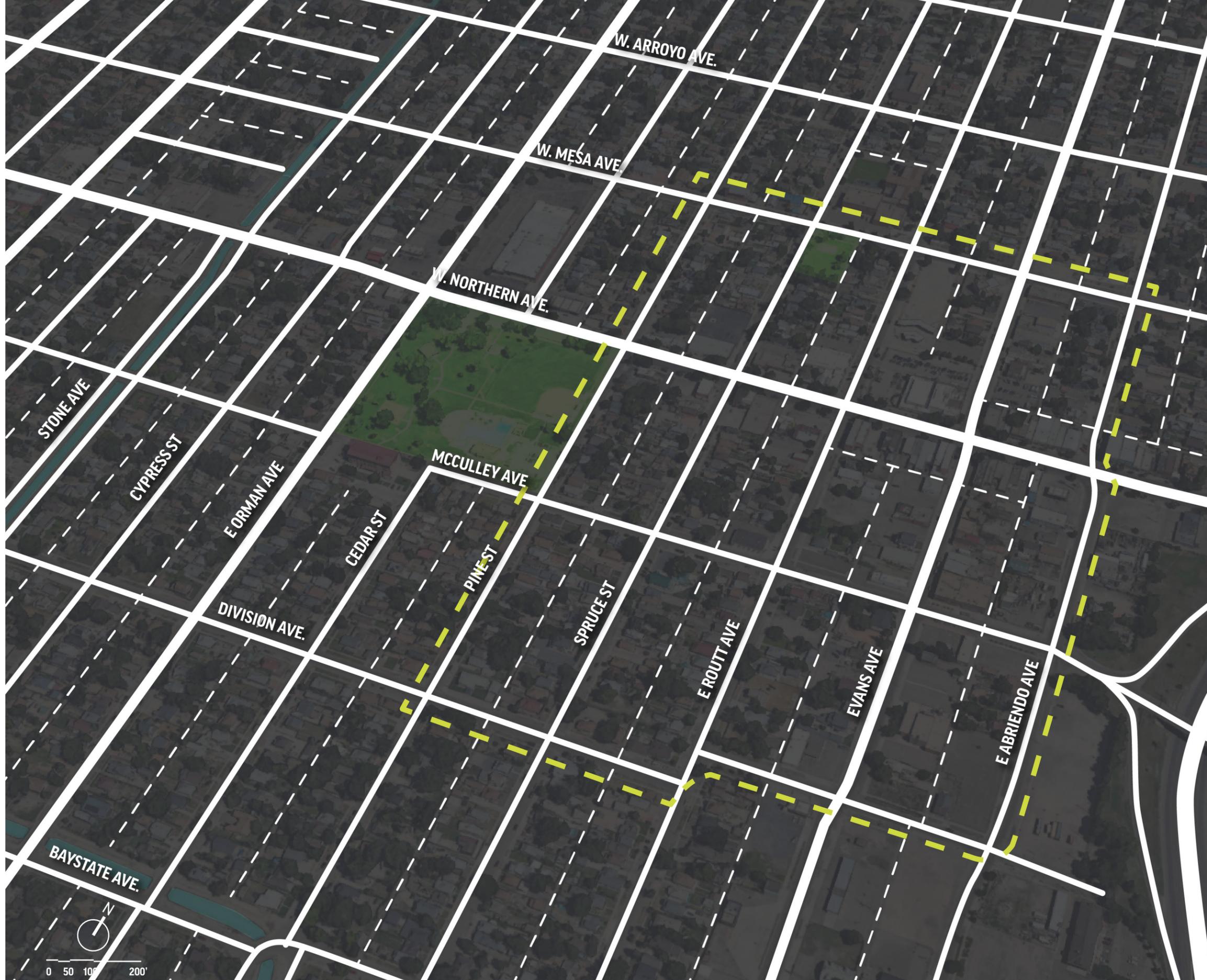
Grid Block Pattern

Block Size: approx. 3.8 acres through 4.2 acres

Block Dimensions: approx. 294' x 620' through 287' x 577'

- Street Network
- Trails
- - - Alley Network
- - - Example Boundary
- Parks & Open Space

This long, narrow grid pattern has rectangular blocks with alleys for parking access, a variety of residential housing types, and a connected network of sidewalks. The grid street pattern supports well-connected neighborhoods and commercial areas for active transportation, transit, and vehicular transportation.



Typical Open Space Typologies

The Bessemer neighborhood has a somewhat limited variety of open space types. Bessemer park is highly activated with many recreational options, and two school parks within the neighborhood offer some (but limited) public access. The canal is not accessible to pedestrians.

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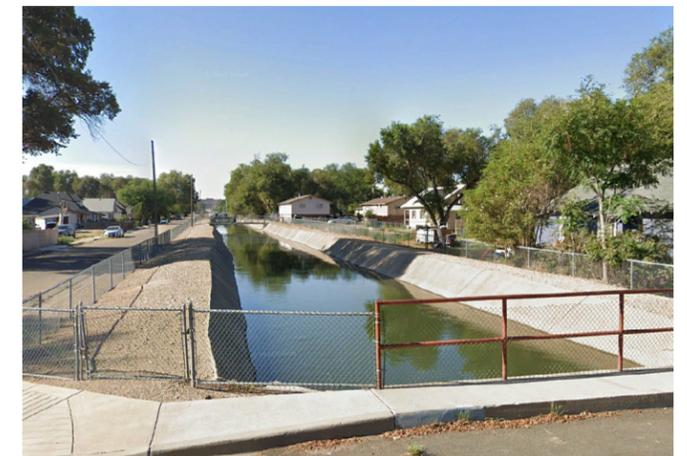
Bessemer Park
Park Type: Neighborhood Park
Size: 7.2 acres



Bessemer School Park
Park Type: School Park
Size: 0.4 acres



Bessemer School Park (2)
Park Type: School Park
Size: 0.6 acres



Canal
Park Type: Natural
Size: N/A

Typical Street Typologies

The Bessemer neighborhood's streets fall into several functional classes. Roadway widths range from 35' to 70'. Residential streets have wide streetscape amenity zones and on-street parking on both sides. All streets have a well-connected sidewalk network throughout the neighborhood.

Note: Roadway widths are measured from curb edge to curb edge.



West Northern Avenue

Functional Class: Major Arterial
 Roadway Width: 70'
 Multimodal Feature(s): Buffered Sidewalks

Sidewalk	Amenity	Parking	Travel Lane	Travel Lane	Turn Lane	Travel Lane	Travel Lane	Parking	Amenity	Sidewalk
9.5'	9.5'	8'	11'	11'	11'	10'	11'	8'	6.5'	8'
Roadway										
70'										



Division Avenue

Functional Class: Local
 Roadway Width: 45'
 Multimodal Feature(s): Buffered Sidewalks

Sidewalk	Buffer	Parking	Travel Lane	Parking	Buffer	Sidewalk
6'	8'	8.5'	28'	8.5'	8.5'	5'
Roadway						
45'						



East Orman Avenue

Functional Class: Minor Arterial
 Roadway Width: 43'
 Multimodal Feature(s): Bike Lane, Buffered Sidewalks

Sidewalk	Buffer	Parking	Bike Lane	Travel Lane	Travel Lane	Bike Lane	Parking	Buffer	Sidewalk
5'	7'	7'	5'	9.5'	9.5'	5'	7'	9'	3.5'
Roadway									
43'									



Pine Street

Functional Class: Local
 Roadway Width: 35'
 Multimodal Feature(s): Buffered Sidewalks

Sidewalk	Buffer	Parking	Travel Lane	Parking	Buffer	Sidewalk
6'	8.5'	8'	19'	8'	8.5'	6'
Roadway						
35'						

Street Design Elements

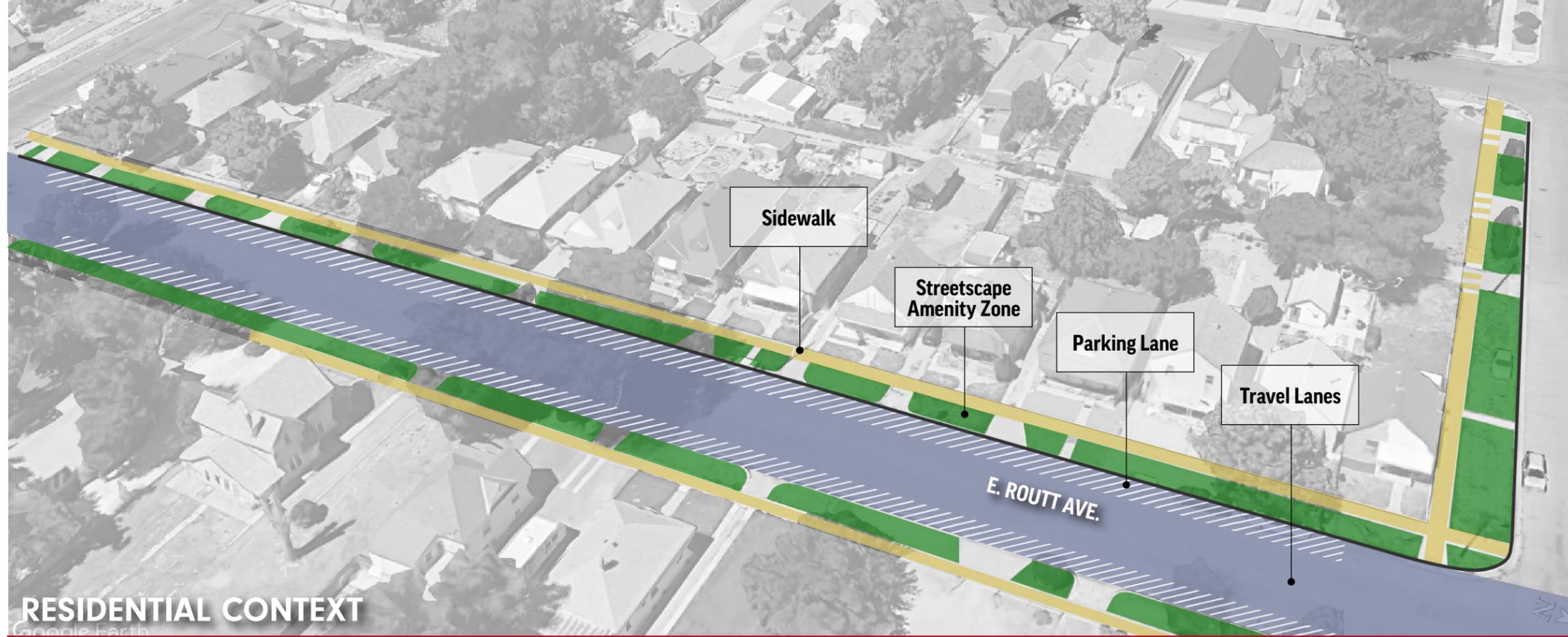
Street Design Elements

- Sidewalk
- Bike Lane
- Streetscape Amenity Zone
- Parking
- Travel Lane

Street Design & Transition

E. Routt Ave. is one example of a street in this pattern that transitions from a residential context to a non-residential context. In its residential context, it has a continuous streetscape amenity zone. As the street moves into non-residential areas, the roadway width is increased and the streetscape amenity zone changes from continuous landscaping into tree wells more suited to the non-residential context.

Contexts	Residential	Non-residential
Roadway Width	42.5'	42.5' - 47.5'
Roadway	On-street parking	On-street parking: parallel and angled
Streetscape	Wide landscape zone on both sides of streets	Tree wells



RESIDENTIAL CONTEXT



NON-RESIDENTIAL CONTEXT

Typical Urban Design Elements

Urban Design Elements

- Front Building Line
- Sidewalk
- Access (Driveways & Parking)
- Streetscape Amenity Zone

Front building lines in this development pattern form a consistent frontage. There is good sidewalk presence, although shared driveways or increased alley access could reduce sidewalk interruptions from driveways. Side driveways, relatively common despite the alley, typically lead to rear detached garages, front-loaded garage, or parking areas located to the rear or side of lots. The streetscape amenity zone buffers sidewalk users from the roadway, but could be improved by increasing street lights, benches, shade, and other amenities that create a safe and comfortable environment for active transportation.



Example Block Location



Typical Urban Design Elements

A series of diagrams shows a closer look at a portion of the example block shown in plan view on the previous page.

Front Building Line

The example block has a well-established frontage to connect to the sidewalks.

Access (Driveway & Parking)

The block's alley helps to reduce the need for front driveways, enhancing the pedestrian experience on sidewalks.



Typical Urban Design Elements

Sidewalk

The example block has continuous sidewalks around the whole block but has some interruptions from private driveways.



Streetscape Amenity Zone

The example block has a wide streetscape amenity zone, ranging from 7' to 9' wide.



Frontage Types

The neighborhood has a variety of entry feature types that help to establish the relationship between the private development and the public realm. Alleys help to reduce the driveway access but there are still some front driveways lead to the rear detached garage or open site for parking. Porches and front walks leading to the sidewalk are common.

Frontage & Access

- Entry Features
- Access (Driveways & Parking)



Entry Features: Porch | Access Format: Alley & Front Driveway
Parking Format: Rear Detached Garage



Entry Features: Portico & Stoop | Access Format: Alley & Front Driveways
Parking Format: Rear Detached Garage & Rear Open Site



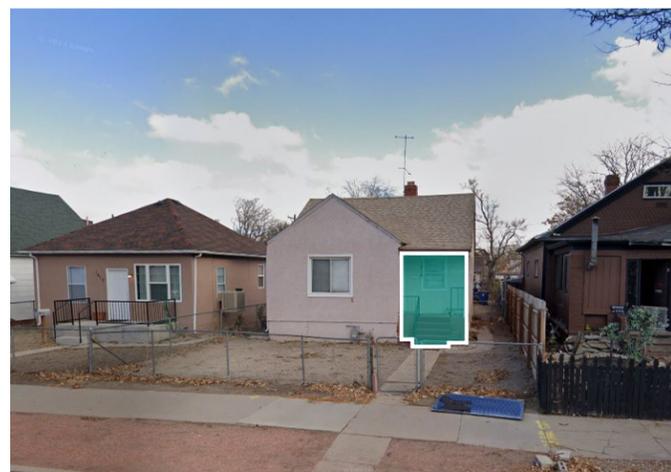
Entry Features: Recessed Porch | Access Format: Alley
Parking Format: Rear Open Site



Entry Features: Porch & Portico | Access Format: Alley
Parking Format: Rear Detached Garage & Rear Open Site



Entry Features: Porch (wrapped) | Access Format: Alley & Side Driveways
Parking Format: Rear Open Site & Detached Garage



Entry Features: Recessed & Stoop | Access Format: Alley
Parking Format: Rear Detached Garage



Entry Features: Portico | Access Format: Side Driveway
Parking Format: Side Driveway



Entry Features: Porch | Access Format: Side Driveway
Parking Format: Rear Detached Garage

Building Types

The Bessemer neighborhood includes a variety of single-family detached housing types at different scales. The lot widths range from 20' wide narrow lots to 45'+ standard lots. The neighborhood also includes some duplexes and multi-unit housing types.



Detached House (Standard Lot)

Scale: Small | Building Footprint: approx. 2,100 ft²
 Lot Size: approx. 4,340 ft² | Height: 1 story | Frontage Type: Neighborhood Yard



Detached House (Small Lot)

Scale: Small | Building Footprint: approx. 1,370 ft²
 Lot Size: approx. 2,860 ft² | Height: 1 story | Frontage Type: Neighborhood Yard



Multi-unit House (9 Units)

Scale: Medium | Building Footprint: approx. 4,290 ft²
 Lot Size: approx. 12,100 ft² | Height: 2.5 stories | Frontage Type: Neighborhood Yard



Detached House (Narrow Lot)

Scale: Small | Building Footprint: approx. 1,190 ft²
 Lot Size: approx. 2,500 ft² | Height: 1 story | Frontage Type: Neighborhood Yard



Duplex

Scale: Small | Building Footprint: approx. 2,125 ft²
 Lot Size: approx. 6,800 ft² | Height: 2 stories | Frontage Type: Neighborhood Yard



Apartment Building

Scale: Small | Building Footprint: approx. 2,400 ft²
 Lot Size: approx. 6,900 ft² | Height: 2.5 stories | Frontage Type: Buffer



Residential Context 03.

Mesa Junction Neighborhood

Historic Square Mixed-Use District

--- Development Pattern Example Area

The Mesa Junction historic neighborhoods are located southwest of Pueblo's Downtown and were developed in a traditional "grid" development pattern. The historic area has a distinctive square block pattern, and is offset by 45 degrees from surrounding north-south oriented blocks.



Block Pattern

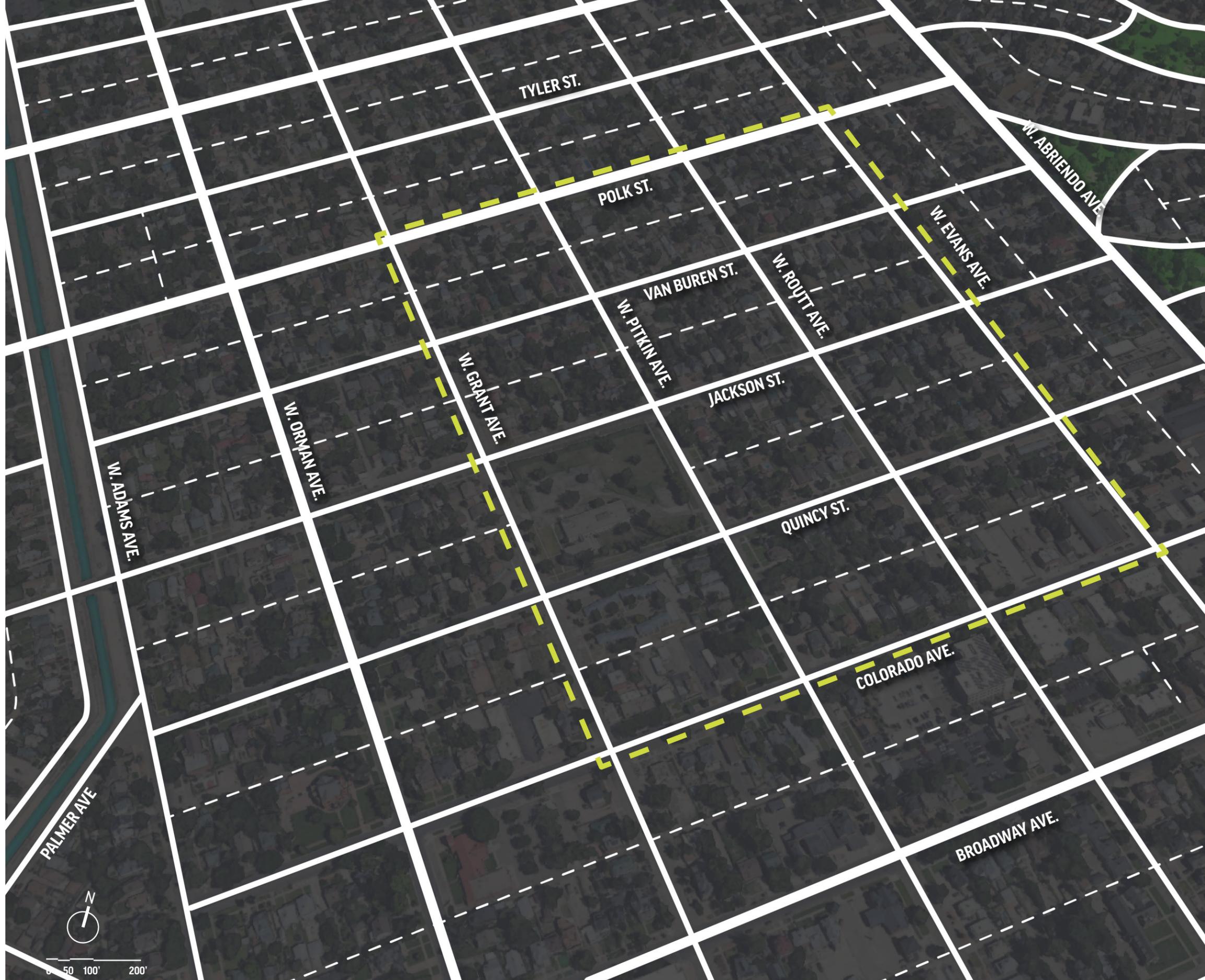
Grid Block Pattern

Block Size: approx. 4.7 acres

Block Dimensions: approx. 452' x 452'

- Street Network
- ● ● Trails
- - - Alley Network
- - - Example Boundary
- Parks & Open Space

The 452' by 452' square blocks provide for flexibility of lot arrangements and alleys for parking access. This square grid pattern is characterized by a variety of residential housing types and a highly-connected network of sidewalks.



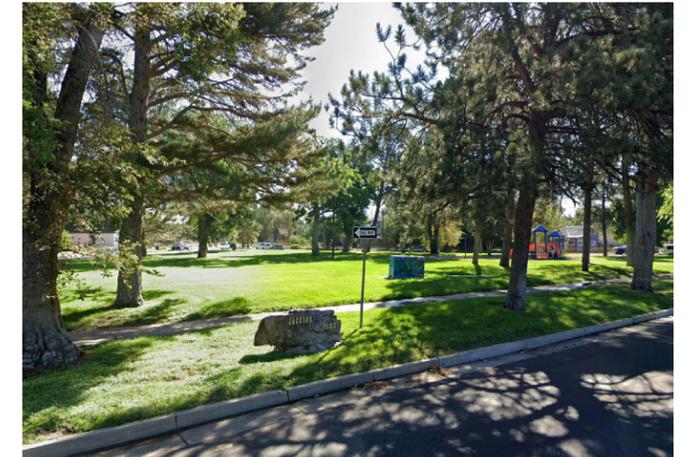
Typical Open Space Typologies

The Mesa Junction neighborhood has a variety of open spaces, ranging from a small-scale neighborhood square to the small and medium-sized neighborhood parks, to a canal corridor.

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Bruner Park
Park Type: Neighborhood Park
Size: 3.6 acres



Jackson Park
Park Type: Neighborhood Park
Size: 1.3 acres



Gateway
Park Type: Neighborhood Square
Size: 0.2 acres



Canal
Park Type: Natural
Size: N/A

Typical Street Typologies

Streets in the Mesa Junction neighborhood has several functional classes. The roadway width ranges from 49' to 57'. Most of the streets within residential areas have a wide streetscape amenity zone and on-street parking. However, the major arterial street that connects Mesa Junction to Downtown does not have a streetscape amenity zone. All streets have well-connected sidewalks.

Note: Roadway widths are measured from curb edge to curb edge.



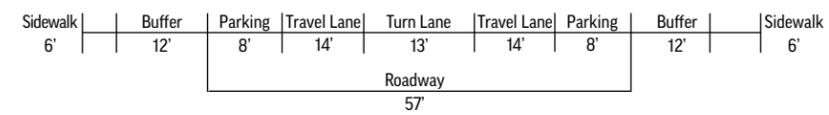
Lincoln Street

Functional Class: Major Arterial
Roadway Width: 49'
Multimodal Feature(s): N/A



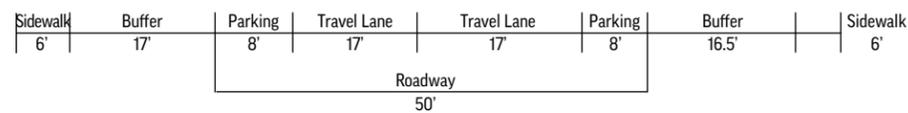
Colorado Avenue

Functional Class: Collector
Roadway Width: 57'
Multimodal Feature(s): Buffered Sidewalks



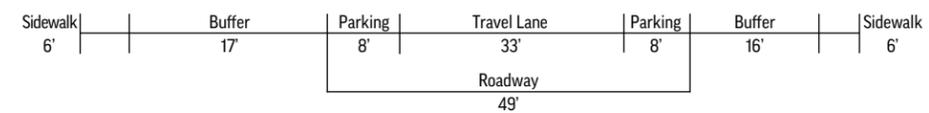
Polk Street

Functional Class: Minor Arterial
Roadway Width: 50'
Multimodal Feature(s): Buffered Sidewalks



West Pitkin Avenue

Functional Class: Local
Roadway Width: 49'
Multimodal Feature(s): Buffered Sidewalks



Street Design Elements

Street Design Elements

- Sidewalk
- Bike Lane
- Streetscape Amenity Zone
- Parking
- Travel Lane

Street Design & Transition

West Roult Avenue is one example of a street in this pattern that transitions from a residential context to a non-residential context. In the residential context, it has a continuous streetscape amenity zone and parallel on-street parking. As the street moves into non-residential areas, the streetscape amenity zone changes into to a hardscape to suit the non-residential context.

Contexts	Residential	Non-residential
Roadway Width	49'	49'
Roadway	On-street parking on both sides of streets	On-street parking: parallel and angle
Streetscape	Wide landscape buffer on both side of streets	Landscape buffer and hardscape around commercial pad site.



RESIDENTIAL CONTEXT



NON-RESIDENTIAL CONTEXT

Typical Urban Design Elements

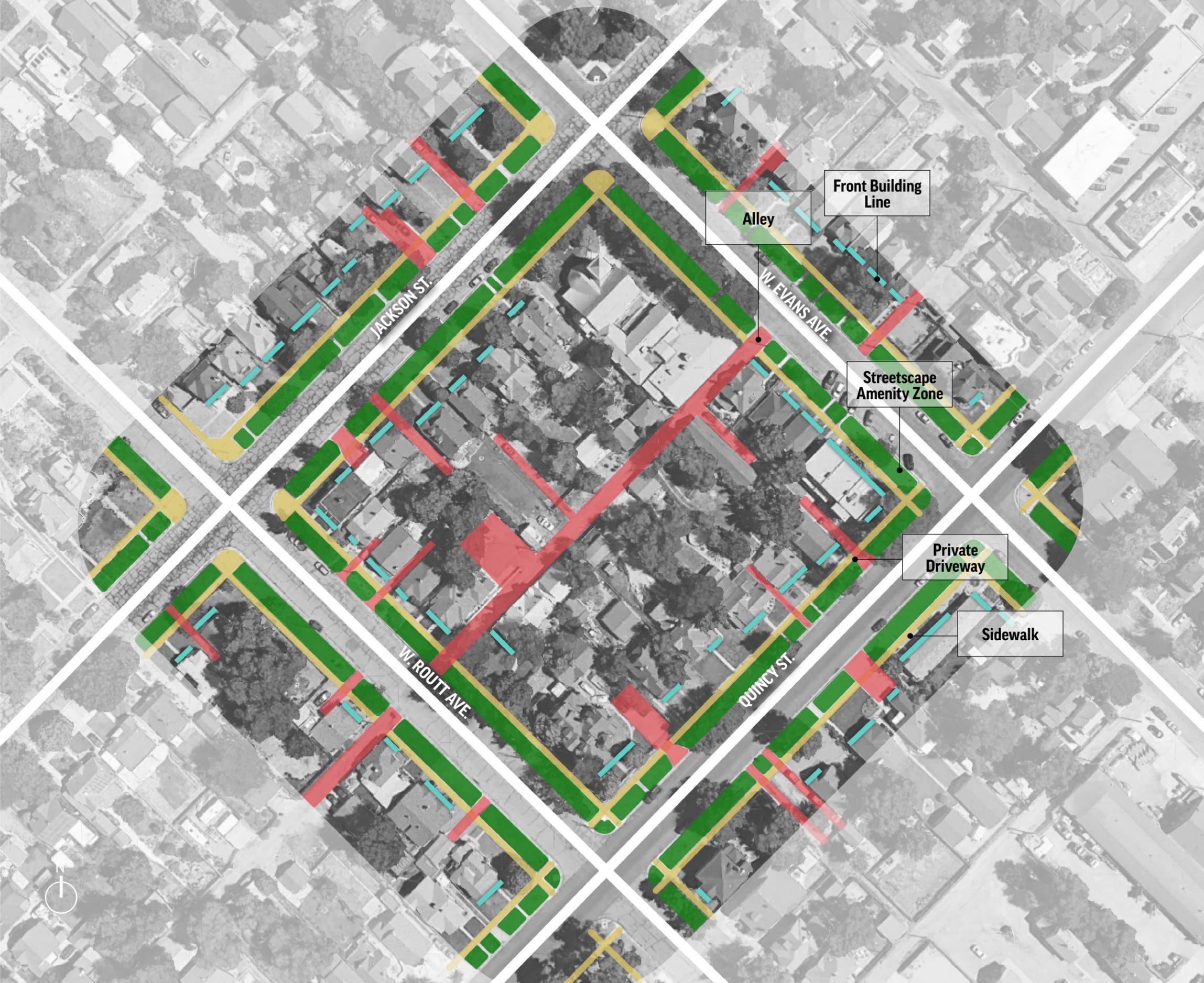
Urban Design Elements

- Front Building Line
- Sidewalk
- Access (Driveways & Parking)
- Streetscape Amenity Zone

Front building lines in this development pattern form a consistent pattern. There is good sidewalk presence, although shared driveways or increased alley access could reduce sidewalk interruptions from driveways. Driveways typically lead to rear detached garages, or parking areas located to the rear or side of lots. The streetscape amenity zone buffers sidewalk users from the roadway, but could be improved by increasing street lights, benches, shade, and other amenities that create a safe and comfortable environment for active transportation.



Example Block Location



Typical Urban Design Elements

A series of diagrams shows a closer look at a portion of the example block shown in plan view on the previous page.

Front Building Line

The example block has a well-established frontage to connect to the sidewalks.

Access (Driveway & Parking)

The block's alley helps to reduce the need for front driveways, enhancing the pedestrian experience on sidewalks.



Typical Urban Design Elements

Sidewalk

The example block has continuous sidewalks around the whole block but has some interruptions from private driveways.



Streetscape Amenity Zone

The example block has a very side streetscape amenity zone, about 16'.

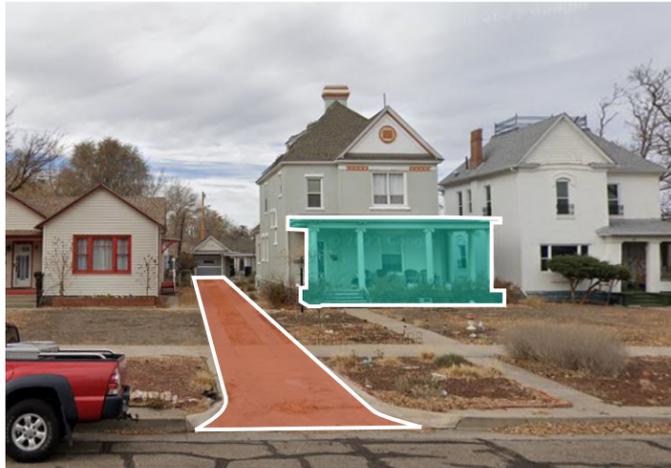


Frontage Types

The neighborhood has a variety of entry feature types that help to establish the relationship between the private development and the public realm. Alleys help to reduce the driveway access but there are still some private driveways that lead to rear parking. Porches and front walks leading to the sidewalk are common.

Frontage & Access

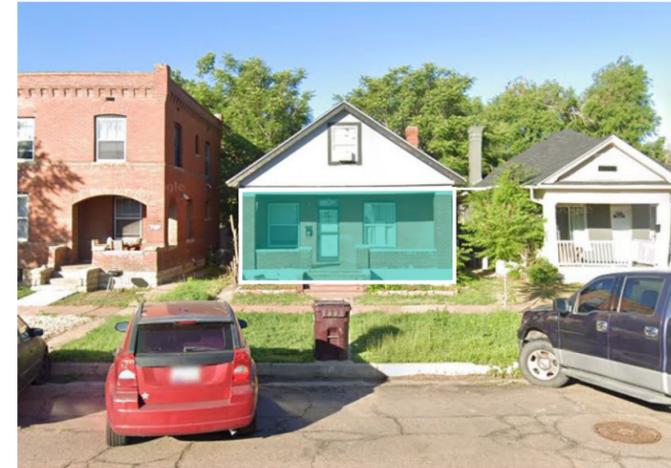
- Entry Features
- Access (Driveways & Parking)



Entry Features: Porch | Access Format: Alley & Side Driveways
Parking Format: Rear Detached Garage



Entry Features: Portico & Stoop | Access Format: Alley
Parking Format: Rear Attached Garage



Entry Features: Recessed Porch | Access Format: Shared Driveways
Parking Format: Rear Detached Garage



Entry Features: Porch & Portico | Access Format: Alley & Front Driveways
Parking Format: Rear Detached Garage



Entry Features: Porch | Access Format: Alley & Side Driveway
Parking Format: Side Car-Port



Entry Features: Stoop | Access Format: Front
Parking Format: Front Parking



Entry Features: Slope Porch, Portico, & Stoop | Access Format: Alley
Parking Format: Rear Detached Garage & Open Site

Building Types

The Mesa Junction neighborhood includes a variety of single-family detached housing types at different scales. The lot widths range from 20' wide narrow lots to 55' standard lots. It neighborhood also includes some duplexes, multi-unit housing, and small apartment buildings.



Detached House (Standard Lot)

Scale: Small | Building Footprint: approx. 1,730 ft²
 Lot Size: approx. 8,280 ft² | Height: 1.5 stories | Frontage Type: Neighborhood Yard



Detached House (Small Lot)

Scale: Small | Building Footprint: approx. 1,250 ft²
 Lot Size: approx. 2,830 ft² | Height: 1 Story | Frontage Type: Neighborhood Yard



Duplex

Scale: Medium | Building Footprint: approx. 2,300 ft²
 Lot Size: approx. 9,535 ft² | Height: 2 stories | Frontage Type: Neighborhood Yard



Multi-Unit House (3 Units)

Scale: Small | Building Footprint: approx. 1,740 ft²
 Lot Size: approx. 5,050 ft² | Height: 2.5 Stories | Frontage Type: Neighborhood Yard



Detached House (Narrow Lot)

Scale: Small | Building Footprint: approx. 600 ft²
 Lot Size: approx. 5,065 ft² | Height: 1.5 Story | Frontage Type: Neighborhood Yard



Duplex

Scale: Small | Building Footprint: approx. 2,400 ft²
 Lot Size: approx. 6,210 ft² | Height: 1 Story | Frontage Type: Neighborhood Yard



Complex

Scale: Large | Building Footprint: approx. 9,530 ft²
 Lot Size: approx. 3,060 ft² | Height: 1 Story | Frontage Type: Buffer



Apartment Building

Scale: Small | Building Footprint: approx. 3,740 ft²
 Lot Size: approx. 9,220 ft² | Height: 2 Stories | Frontage Type: Street-Front



Non-Residential Context 01.

Downtown, Union & Main District

Historic Downtown Business District

--- Development Pattern Example Area

Union Ave. and Main St. are two major historic commercial corridors in Pueblo's historic downtown business district. It is formed by a traditional "grid" development pattern, offset by 45 degrees from the north-south block pattern of surrounding areas.



Block Pattern

Grid Block Pattern

Block Size: approx. 3.4 acres

Block Dimensions: approx. 338' x 434'

- Street Network
- Trails
- - - Example Boundary
- Parks & Open Space
- ||||| Rail

The area's block pattern features small block sizes, alleys for parking and service access, a variety of commercial uses, and a connected network of sidewalks. Despite some surface parking, the grid street pattern supports well-connected commercial areas for active transportation, transit, and vehicular transportation.



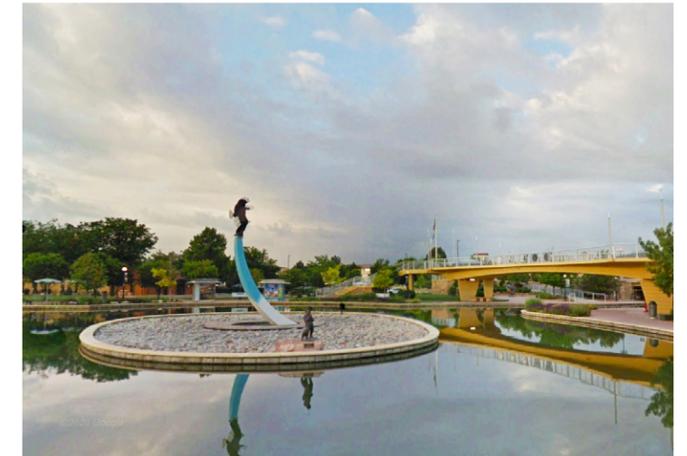
Typical Open Space Typologies

The Historic Arkansas Riverwalk is the significant open space around the historic business district. The East Riverwalk provides outdoor event plazas, stages, walkways, and boat rides. The Pueblo Ice Arena is the indoor recreational open space on the north of the example block area. Additionally, there is a trail corridor along the Arkansas River to the west.

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East Riverwalk
Park Type: Specialized Facilities
Size: N/A



Pueblo Riverwalk
Park Type: Specialized Facilities
Size: N/A



Pueblo Ice Arena
Park Type: Specialized Facilities
Size: 1.5 acres



Arkansas River
Park Type: Open Space
Size: N/A

Typical Street Typologies

Streets in the historic district fall into several functional classes. The roadway width ranges from 48' to 64'. Most of the streets have a streetscape amenity zone with lighting, benches, planters, and street trees and parking on both sides of the streets. The primary commercial corridor, S. Union Ave., features dedicated bike lanes on both sides of the street. All streets have sidewalks and are well-connected throughout the district.

Note: Roadway widths are measured from curb edge to curb edge.



Sidewalk	Amenity	Travel Lane	Amenity	Sidewalk				
6'	4'	14'	13.5'	12'	12.5'	12'	6'	9.5'
Roadway 64'								

W. City Center Dr.

Functional Class: Major Arterial
Roadway Width: 64'
Multimodal Feature(s): Buffered sidewalk



Sidewalk +Amenity	Parking	Bike	Buffer	Travel Lane	Travel Lane	Buffer	Bike	Parking	Amenity + Sidewalk
11'	8'	6'	3'	12'	12'	3'	6'	8'	10.5'
Roadway 58'									

Alan Hamel Avenue

Functional Class: Minor Arterial
Roadway Width: 58'
Multimodal Feature(s): Buffered Bike Lane, Buffered Sidewalks



Sidewalk	Angle Parking	Travel Lanes	Angle Parking	Sidewalk
6'	16'	31'	16'	6'
Roadway 63'				

East D Street

Functional Class: Collector
Roadway Width: 63'
Multimodal Feature(s): Buffered Sidewalks



Sidewalk	Amenity	Parking	Bike Lane	Travel Lane	Center Turn Lane	Travel Lane	Bike Lane	Parking	Amenity	Sidewalk
5'	3.5'	7.5'	5.5'	11'	11'	11'	5.5'	7.5'	3.5'	5'
Roadway 59'										

S. Union Ave.

Functional Class: Major Arterial
Roadway Width: 59'
Multimodal Feature(s): Bike Lane, Buffered sidewalk



Sidewalk	Amenity	Angle Parking	Travel Lanes	Parking	Amenity	Sidewalk
7'	5'	17'	24'	8'	5'	7'
Roadway 48'						

West B Street

Functional Class: Local
Roadway Width: 48'
Multimodal Feature(s): Buffered Sidewalks

Street Design Elements

Street Design Elements

- Sidewalk
- Bike Lane
- Streetscape Amenity Zone
- Parking
- Travel Lane

Street Design & Transition

Union Ave is one example of a street in this pattern that transitions between differing non-residential contexts, and in response, the street design alters. The S. Union Ave. segment has bike lanes, on-street parking, street amenity zones, and wide sidewalks. However, the N. Union Ave segment street design alters to favor vehicular transportation: the bike lane is only kept on one side of the street, although the roadway width is increased; and the street amenity zone is discontinued on one side of the street.

Contexts	Non-Residential A	Non-residential B
Roadway Width	59'	66'
Roadway	On-street parking on both sides of streets. Bike lanes on both side of streets.	On-street parking on both sides of streets. Bike lane on one side. Center median with trees and shrubs.
Streetscape	Planted streetscape Amenity zone. Curb extensions at intersections and mid-block bump-outs to help narrow the street and provide space for green.	Mid-block bump-outs on one side provide landscape amenity zone and define the on-street parking area.



Typical Urban Design Elements

Urban Design Elements

- Front Building Line
- Sidewalk
- Access (Driveways & Parking)
- Streetscape Amenity Zone
- Curb Cut

Front building lines in this development pattern form a consistent frontage. The alleys help limit the need for driveways, and allow parking areas to be located to the rear or side of lots. There is a good sidewalk presence. The streetscape amenity zone buffers sidewalk users from the roadway and provides street lights, benches, shade, and other amenities that create a safe and comfortable environment for active transportation.



Example Block Location

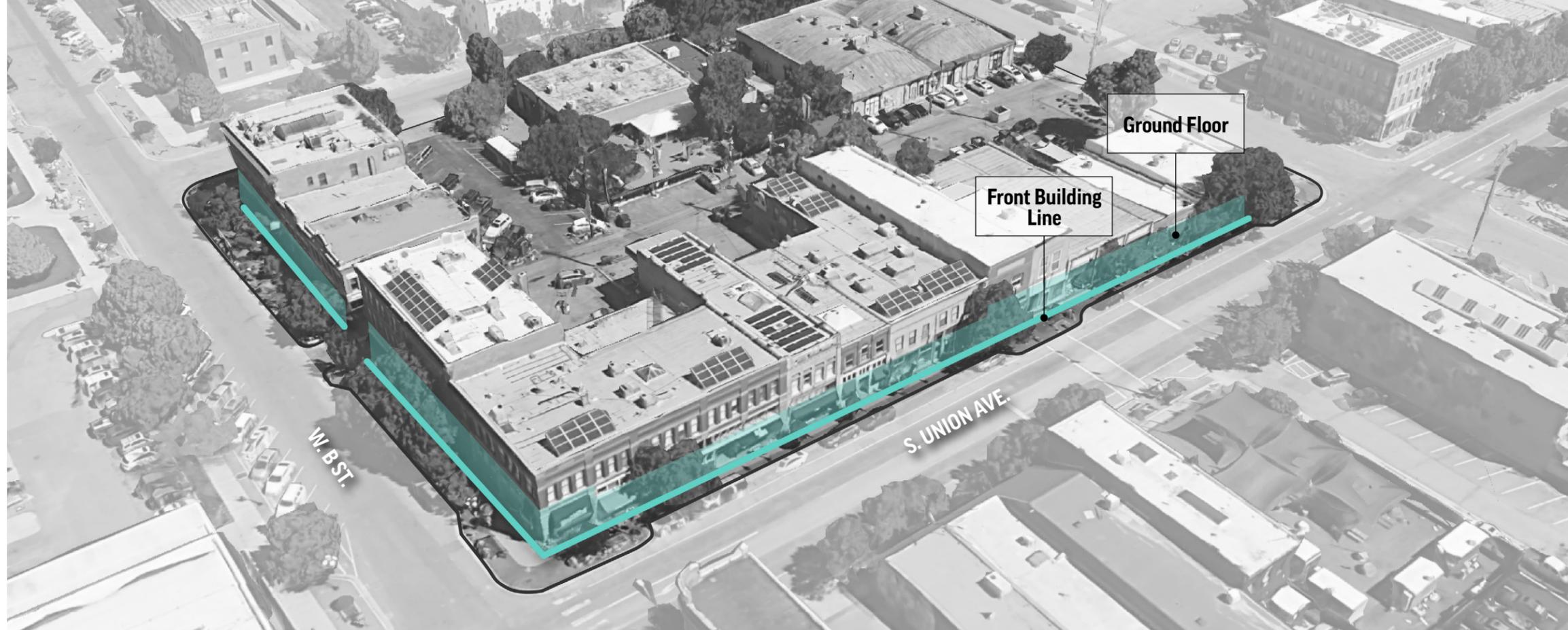


Typical Urban Design Elements

A series of diagrams shows a closer look at a portion of the example block shown in plan view on the previous page.

-  Ground Floor
-  Front Building Line

The example block has a well-established frontage to connect to the sidewalks.



-  Streetscape Amenity Zone

The example block has a streetscape amenity zone that provides a full range of pedestrian amenities, including street lights, benches, planters, and street trees.

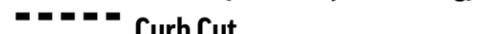


Typical Urban Design Element

-  Sidewalk
-  Curb Cut

The example block has continuous sidewalks with very limited curb cuts.



-  Access (Driveway & Parking)
-  Curb Cut

The block's alley helps to reduce the need for front driveways, enhancing the pedestrian experience on the sidewalks. Parking areas are located inside the block, allowing for a highly activated streetscape.



Building Types

The historic business district features a variety of small to medium-scale commercial and mixed-use building types. Buildings were built up to the street and designed to build a relationship with the public realm through architectural design. Highly-transparent ground floors foster an activated streetscape.



Mixed-Use Building

Scale: Small | Building Footprint: approx. 1,748 ft² | Land Use: Commercial
Lot Size: approx. 3,418 ft² | Height: 2 stories | Frontage Type: Street-Front



Mixed Use Building

Scale: Medium | Building Footprint: approx. 11,884 ft² | Land Use: Commercial
Lot Size: approx. 15,325 ft² | Height: 2 stories | Frontage Type: Street Front



High School

Scale: Small | Building Footprint: approx. 12,227 ft² | Land Use: Institutional
Lot Size: approx. 20,981 ft² | Height: 1 story | Frontage Type: Terrace



Commercial

Scale: Small | Building Footprint: approx. 1,699 ft² | Land Use: Commercial
Lot Size: approx. 3,010 ft² | Height: 1 story | Frontage Type: Street-Front



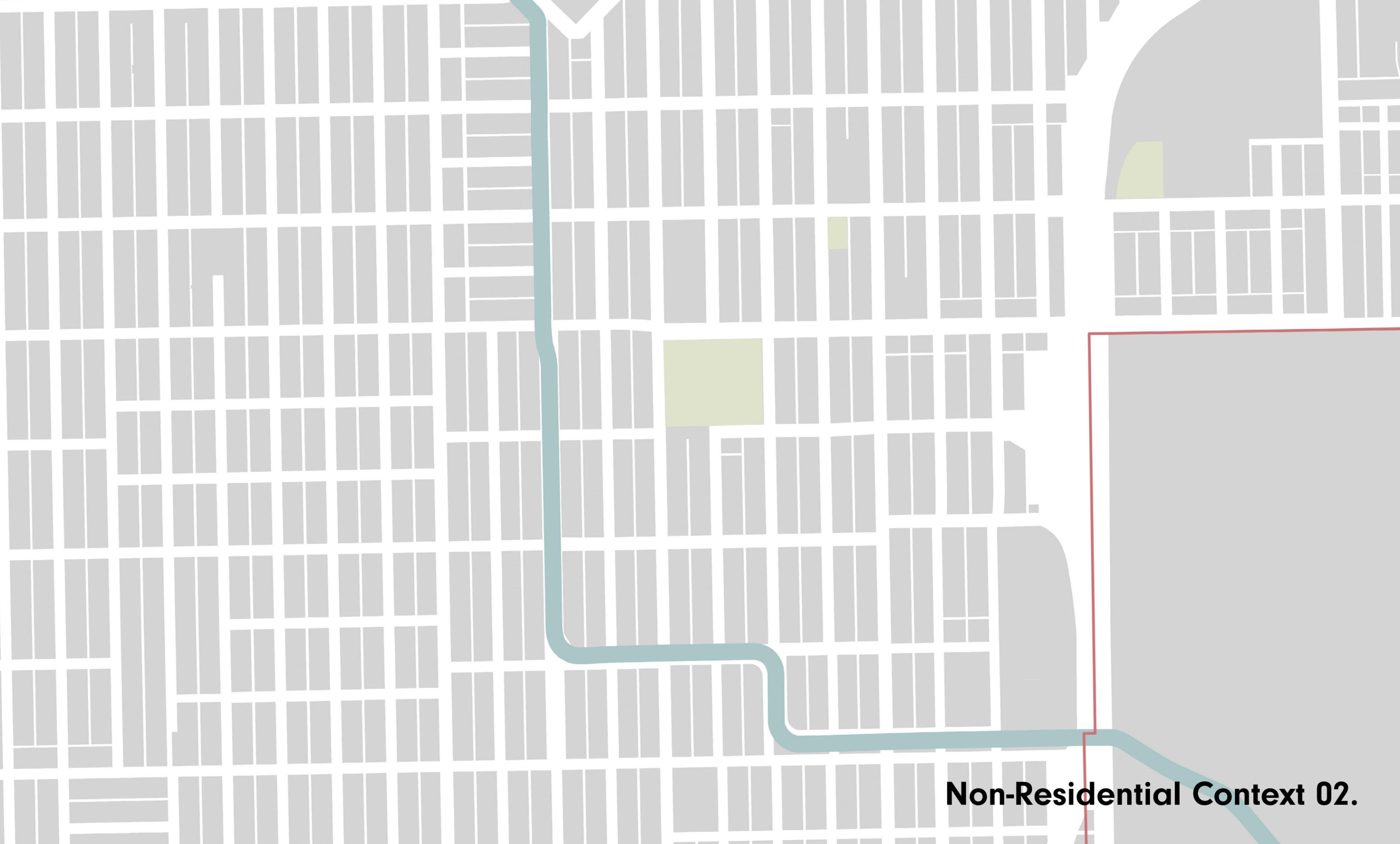
Commercial

Scale: Medium | Building Footprint: approx. 3,882 ft² | Land Use: Commercial
Lot Size: approx. 14,286 ft² | Height: 1 story | Frontage Type: Street-Front



Civic Building

Scale: Large | Building Footprint: approx. 43,498 ft² | Land Use: Government
Lot Size: approx. 46,497 ft² | Height: 3 stories | Frontage Type: Street-Front



Non-Residential Context 02.

Bessemer Neighborhood

Historic Traditional Mixed-Use District

--- Development Pattern Example Area

The Bessemer Neighborhood is historic neighborhood in Pueblo and is unique for its long, narrow block pattern. It has a currently-underutilized historic commercial corridor, and many distinctive historic buildings.



Block Pattern

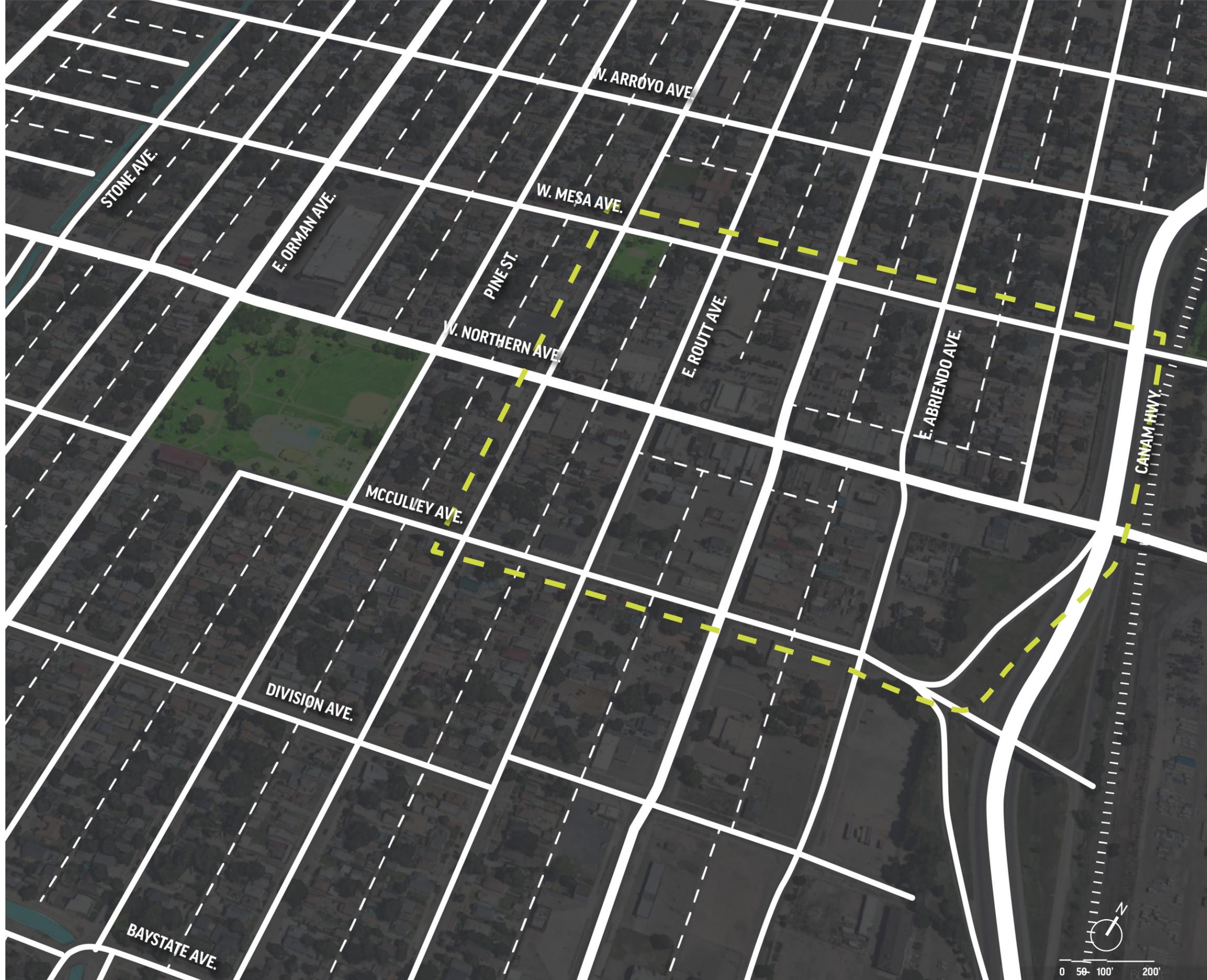
Grid Block Pattern

Block Size: approx. 3.8 acres through 4.2 acres

Block Dimensions: approx. 294' x 620' through 287' x 577'

- Street Network
- Trails
- - - Example Boundary
- Parks & Open Space
- ||||| Rail

This grid pattern is characterized by long, narrow rectangular blocks with alleys for parking and service access, a mix of commercial and residential uses, and a connected network of sidewalks. The grid street pattern supports well-connected commercial for active transportation, transit, and vehicular transportation.



Typical Open Space Typologies

The Bessemer neighborhood has a somewhat limited variety of open space types. Bessemer park is highly activated with many recreational options, and two school parks within the neighborhood offer some (but limited) public access. The canal is not accessible to pedestrians.

Bessemer neighborhood has commercial corridors and residential neighborhoods. This page is the same as the Open Space Typologies page in the Residential section.

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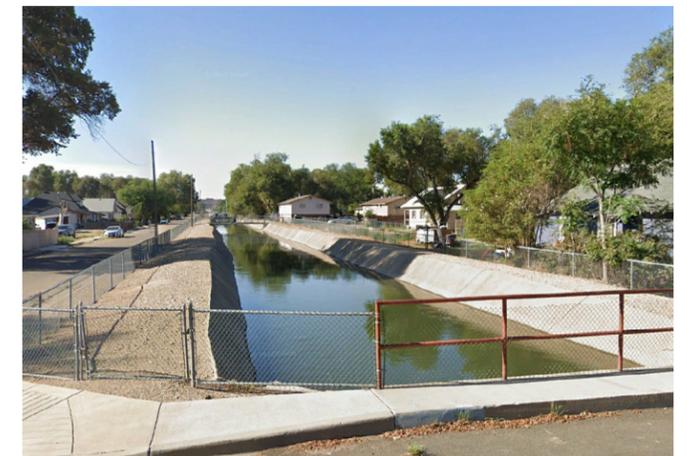
Bessemer Park
Park Type: Neighborhood Park
Size: 7.2 acres



Bessemer School Park
Park Type: School Park
Size: 0.4 acres



Bessemer School Park (2)
Park Type: School Park
Size: 0.6 acres



Canal
Park Type: Natural
Size: N/A

Typical Street Typologies

The Bessemer neighborhood's streets fall into several functional classes. Roadway width ranges from 42' to 70'. The primary commercial corridor, W. Northern Ave., has a wide sidewalk and a streetscape amenity zone with lighting, benches, planters, and street trees. Streets around the example areas have on-street parking at least on one side, and the streetscape changes to accommodate different contexts. All streets have sidewalks and are well-connected throughout the district.

Note: Roadway widths are measured from curb edge to curb edge.



West Northern Avenue

Functional Class: Major Arterial
 Roadway Width: 70'
 Multimodal Feature(s): Buffered Sidewalks

Sidewalk	Amenity	Parking	Travel Lane	Travel Lane	Turn Lane	Travel Lane	Travel Lane	Parking	Amenity	Sidewalk
9.5'	9.5'	8'	11'	11'	11'	10'	11'	8'	6.5'	8'
Roadway										
70'										



Division Ave.

Functional Class: Collector
 Roadway Width: 50'
 Multimodal Feature(s): N/A

Sidewalk	Parking	Travel Lane	Travel Lane	Travel Lane	Sidewalk
6'	11'	10'	10'	19'	6'
Roadway					
50'					



Evans Avenue

Functional Class: Minor Arterial
 Roadway Width: 46'
 Multimodal Feature(s): Buffered Sidewalks

Sidewalk + Amenity	Parking	Travel Lane	Travel Lane	Parking	Amenity + Sidewalk
11'	8'	14'	14'	8'	12'
Roadway					
44'					



E. Routh Ave.

Functional Class: Local
 Roadway Width: 43'
 Multimodal Feature(s): Buffered Sidewalks

Sidewalk	Buffer	Parking	Travel Lane	Travel Lane	Parking	Buffer	Sidewalk
5.5'	9.5'	8'	13'	13'	8'	6.5'	8'
Roadway							
42'							

Street Design Elements

Street Design Elements

- Sidewalk
- Bike Lane
- Streetscape Amenity Zone
- Parking
- Travel Lane

Street Design & Transition

W. Northern Ave. is one example of a street in this pattern that transitions between differing non-residential contexts, and in response, the street design alters. Segment A (to the right) has a streetscape amenity zone with landscape and street furniture, and on-street parking. Toward the west along segment B, the street loses both on-street parking and the streetscape amenity zone.

Context	Non-Residential A	Non-residential B
Roadway Width	70'	52.5'
Roadway	On-street parking on both side of streets.	5 Lanes street.
Streetscape	An amenity strip zone provides landscape and street furniture.	5' sidewalk on both side of streets.



Typical Urban Design Elements

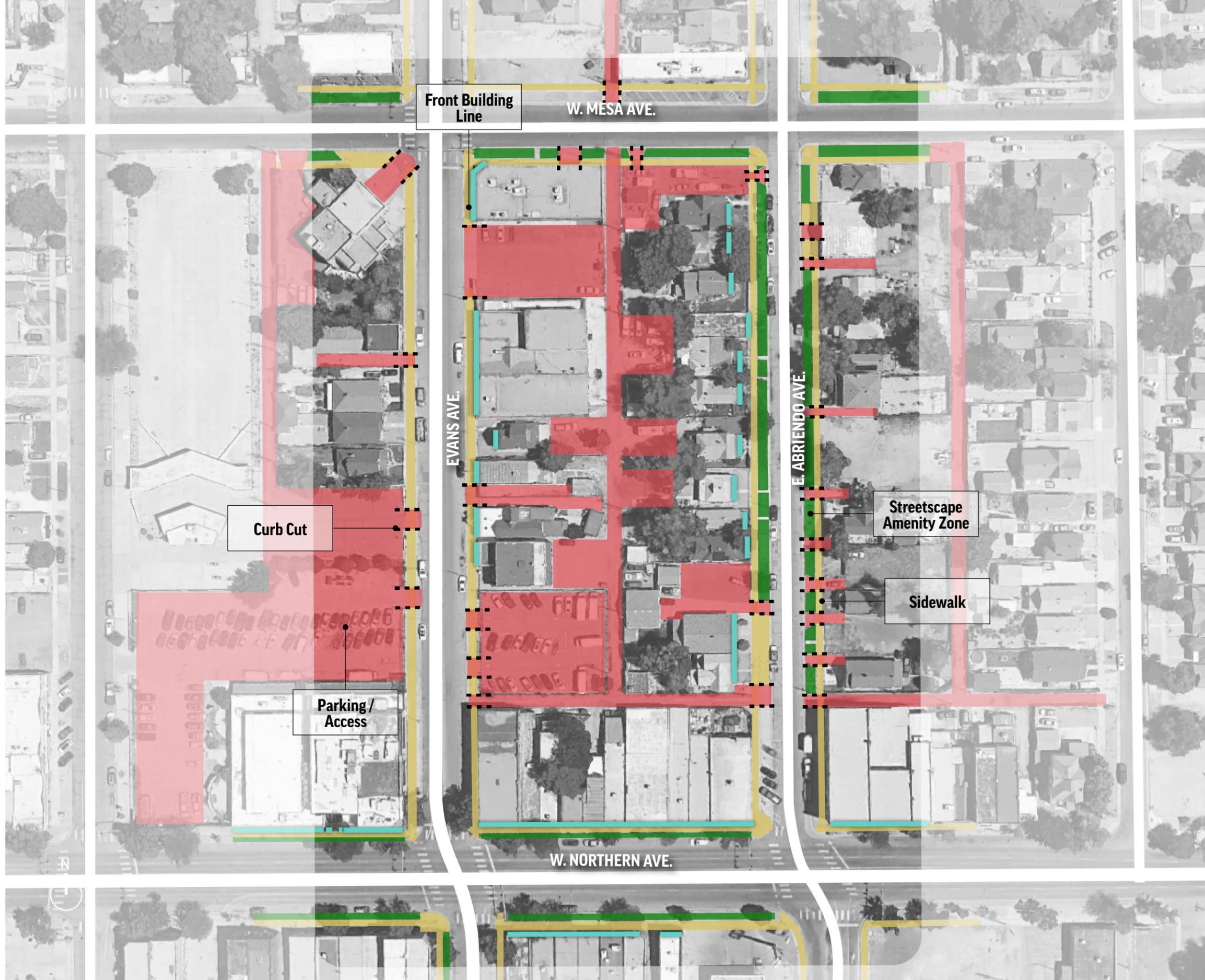
Urban Design Elements

-  Front Building Line
-  Sidewalk
-  Access (Driveways & Parking)
-  Streetscape Amenity Zone
-  Curb Cut

Front building lines in this development pattern are varied due to the mix of uses present. The side facing W. Northern Ave. has established front building lines. Alleys allow parking and service access with minimal curb cuts to the sidewalk. The streetscape amenity zone is inconsistently present, though where it exists it is wide and buffers pedestrians from the roadway.



Example Block Location



Typical Urban Design Elements

A series of diagrams shows a closer look at a portion of the example block shown in plan view on the previous page.

-  Ground Floor
-  Front Building Line

The example block has a well-established frontage to connect to the sidewalks.

Streetscape Amenity Zone

Along the commercial corridor (Northern Ave.), the example block has a streetscape amenity zone that provides street lights, benches, planters, and street trees. Along the residential streets, the streetscape amenity zone only includes landscaping.



Typical Urban Design Elements

-  Sidewalk
-  Curb Cut

The example block has continuous sidewalks around the whole block with minimal interruptions from curb cuts.



-  Access (Driveway & Parking)
-  Curb Cut

The block's alley helps to reduce the need for front driveways, enhancing the pedestrian experience on the sidewalks. Parking areas are located inside the block, allowing for a highly activated streetscape.



Building Types

The Bessemer area has a variety of scales and types of commercial and mixed-use buildings. However, many buildings are underutilized or vacant, even along the pedestrian-friendly commercial corridor core. Auto-oriented commercial emerges along with surface parking areas towards the west and east side.



Mixed Use Building

Scale: Small | Building Footprint: approx. 4,359 ft² | Land Use: Commercial
Lot Size: approx. 4,359 ft² | Height: 2 stories | Frontage Type: Street Front



Mixed Use Building

Scale: Medium | Building Footprint: approx. 7,860 ft² | Land Use: Commercial
Lot Size: approx. 7,860 ft² | Height: 2 stories | Frontage Type: Street Front



Commercial

Scale: Medium | Building Footprint: approx. 8,624 ft² | Land Use: Commercial
Lot Size: approx. 8,624 ft² | Height: 1 story | Frontage Type: Street Front



Commercial

Scale: Medium | Building Footprint: approx. 11,545 ft² | Land Use: Commercial
Lot Size: approx. 11,545 ft² | Height: 1 story | Frontage Type: Street Front



Commercial

Scale: Small | Building Footprint: approx. 2,133 ft² | Land Use: Commercial
Lot Size: approx. 2,133 ft² | Height: 1 story | Frontage Type: Street Front



Commercial

Scale: Small | Building Footprint: approx. 4,820 ft² | Land Use: Commercial
Lot Size: approx. 8,850 ft² | Height: 1 story | Frontage Type: Street Front



Drive-Thru

Scale: Small | Building Footprint: approx. 1,593 ft² | Land Use: Commercial
Lot Size: approx. 9,601 ft² | Height: 1 story | Frontage Type: Buffer



Adult Day Care

Scale: Large | Building Footprint: approx. 20,209 ft² | Land Use: Commercial
Lot Size: approx. 59,497 ft² | Height: 2 stories | Frontage Type: Buffer & Parking



Non-Residential Context 03.

Mesa Junction Neighborhood

Historic Square Mixed-Use District

--- Development Pattern Example Area

The Mesa Junction historic neighborhoods are located southwest of Pueblo's Downtown and were developed in a traditional "grid" development pattern. The historic area has a distinctive square block pattern, and is offset by 45 degrees from surrounding north-south oriented blocks. Several mixed-use blocks create a pedestrian-friendly commercial node and a vibrant corridor.



Block Pattern

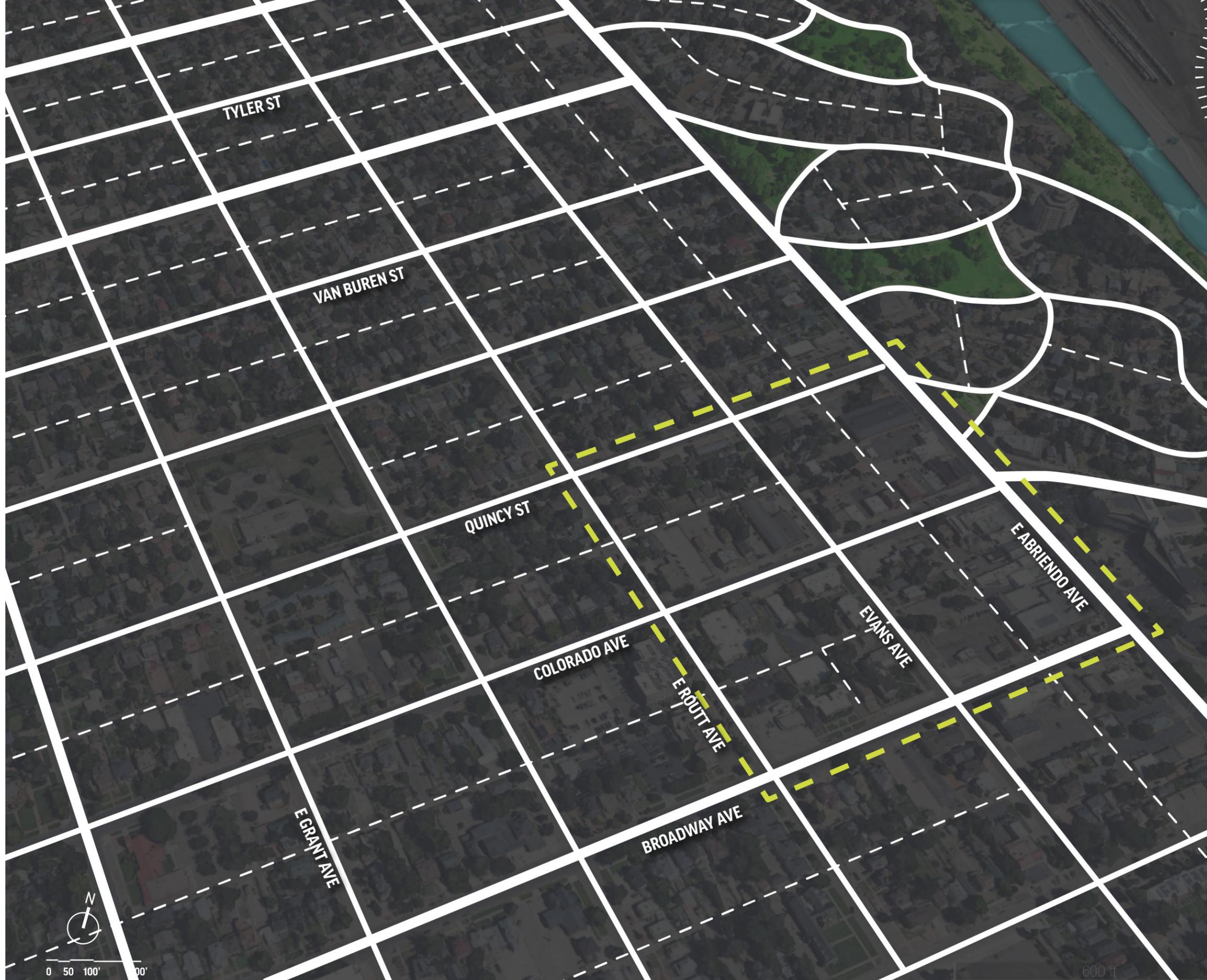
Grid Block Pattern

Block Size: approx. 4.7 acres

Block Dimensions: approx. 452' x 452'

- Street Network
- ● ● Trails
- Example Boundary
- Parks & Open Space
- ||||| Rail

The 452' by 452' square blocks provide flexibility in lot arrangements, alleys for parking access, and different scales of commercial and mixed-use building types. The grid street pattern supports well-connected neighborhoods and commercial areas for active transportation, transit, and vehicular transportation.



Typical Open Space Typologies

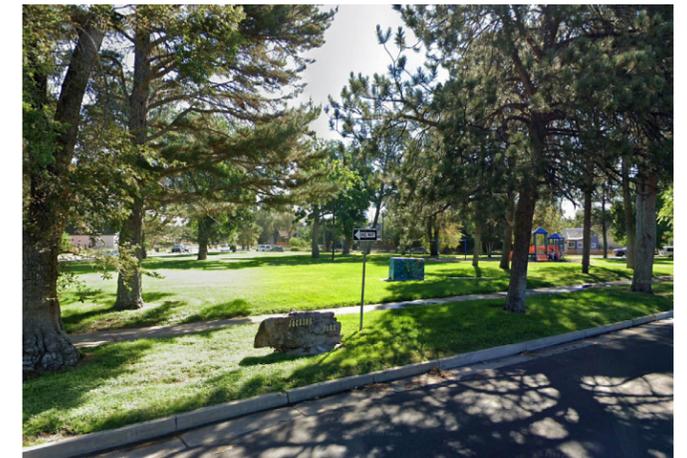
The Mesa Junction neighborhood has a variety of open spaces, ranging from a small-scale neighborhood square to the small and medium-sized neighborhood parks, to a canal corridor.

Mesa Junction has commercial centers and residential neighborhoods. This page is the same as the Open Space Typologies page in the Residential section.

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Bruner Park
Park Type: Neighborhood Park
Size: 3.6 acres



Jackson Park
Park Type: Neighborhood Park
Size: 1.3 acres



Gateway
Park Type: Neighborhood Square
Size: 0.2 acres



Canal
Park Type: Natural
Size: N/A

Typical Street Typologies

The Mesa Junction area streets fall into several functional classes. Roadway widths range from 47' to 115'. The primary northwest-southeast commercial corridor, E. Abriendo Ave., has a decent sidewalk and a streetscape amenity zone with lighting, benches, planters, and street trees. Although the roadway is wide, the on-street parking and angled parking in the middle

of the roadway help to reduce the traffic speeds and provide additional space for green. The two major southwest-northeast streets are more pedestrian-oriented and provide bike lanes on both sides of the streets. Where streets transition into a residential context, the streetscape amenity zone changes to a wide landscaped strip.



Sidewalk + Amenity	Angle Parking	Travel Lane	Travel Lane	Turn Lane / Angle Parking	Median	Angle Parking	Travel Lane	Travel Lane	Parking	Amenity + Sidewalk
9'	14'	19'	14'	12'	8'	14'	13'	13'	8'	9.5'
Roadway										
115'										

East Abriendo Avenue

Functional Class: Major Arterial
 Roadway Width: 115'
 Multimodal Feature(s): Buffered Sidewalks

Note: Roadway widths are measured from curb edge to curb edge.



Sidewalk	Angle Parking	Travel Lane	Travel Lane	Center Turn Lane	Travel Lane	Angle Parking	Sidewalk	
5.5'	11'	11'	11'	11'	20'	12'	8'	
Roadway								
78' / 60'								

Colorado Avenue

Functional Class: Collector
 Roadway Width: 78' / 60'
 Multimodal Feature(s): N/A



Sidewalk	Amenity	Parking	Bike Lane	Travel Lane	Center Turn Lane	Travel Lane	Bike Lane	Parking	Amenity	Sidewalk
5'	3.5'	7.5'	5.5'	11'	11'	11'	5.5'	7.5'	3.5'	5'
Roadway										
59'										

Broadway Avenue

Functional Class: Minor Arterial
 Roadway Width: 59'
 Multimodal Feature(s): Bike Lane, Buffered Sidewalks



Sidewalk	Buffer	Parking	Travel Lane	Travel Lane	Parking	Buffer	Sidewalk
6'	15'	8'	15.5'	15.5'	8'	17'	6'
Roadway							
47'							

West Evans Avenue

Functional Class: Local
 Roadway Width: 47'
 Multimodal Feature(s): Buffered Sidewalks

Street Design Elements

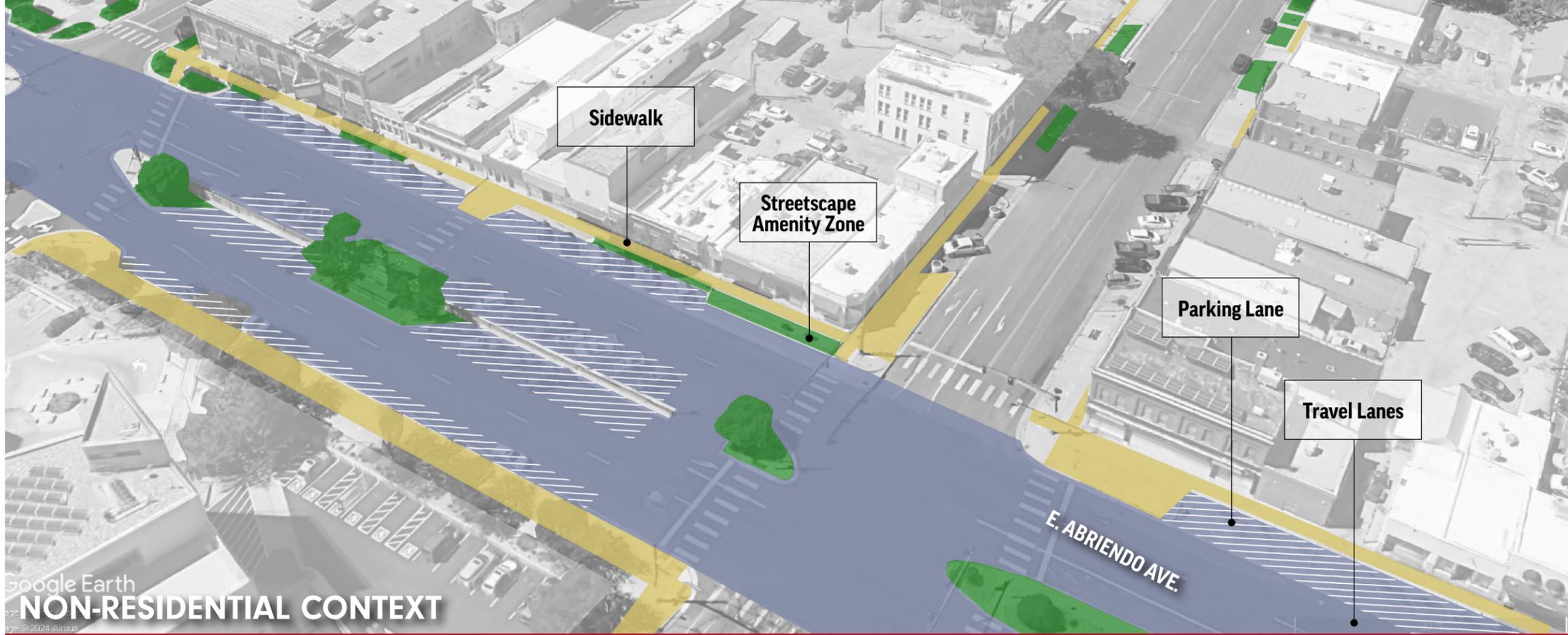
Street Design Elements

-  Sidewalk
-  Bike Lane
-  Streetscape Amenity Zone
-  Parking
-  Travel Lane

Street Design & Transition

E. Abriendo Ave. is one example of a street in this pattern that transitions from a non-residential context into a residential context. In the non-residential context, it has angled on-street parking on both sides of the streets and in the middle of the streets, and a street amenity zone. Despite its wide roadway width, the middle parking areas help to calm traffic speeds. In the residential context, the middle parking area changes to a landscaped median, and the streetscape amenity zone widens.

Contexts	Non-Residential	Residential
Roadway Width	115'	99'
Roadway	Angled on-street parking on both side of streets. Angled parking in the middle of the roadways.	30' wide landscape median. Informal on-street parking on both side of the streets.
Streetscape	An amenity strip zone provides landscape and street furniture. 6' - 9' wide sidewalks.	16' wide continuous landscape buffer zones. 5' sidewalks.



Typical Urban Design Elements

Urban Design Elements

- Front Building Line
- Sidewalk
- Access (Driveways & Parking)
- Streetscape Amenity Zone
- Curb Cut

Front building lines in this development pattern are varied due to the mix of uses present. The side facing E. Abriendo Ave. has consistent front building lines, compared with the other sides of the example block. There is good sidewalk presence and the alleys reduce curb cuts. Driveways are limited on the side facing E. Abriendo Ave., although this pattern breaks down on the other sides of the blocks, as frequent driveways and surface parking faces the residential building opposite. The streetscape amenity zone is consistent and buffers pedestrians from the roadway.



Example Block Location



Typical Urban Design Elements

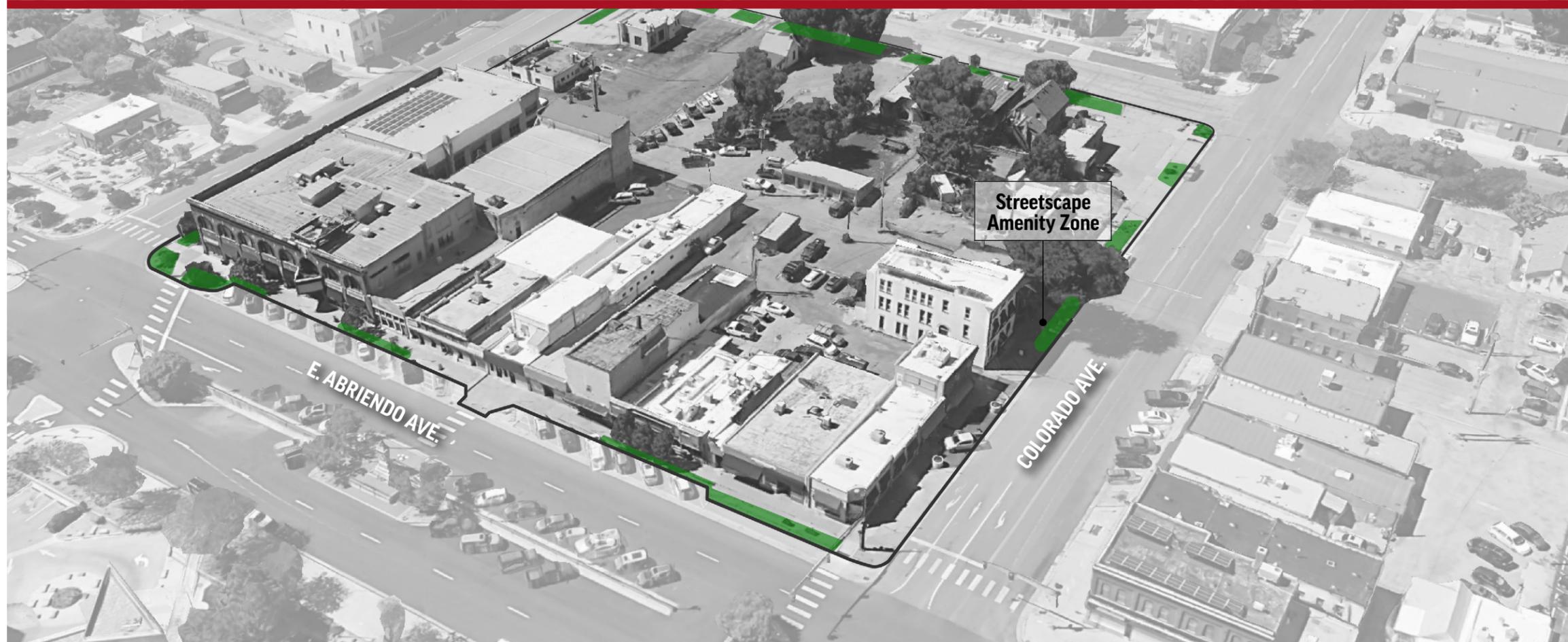
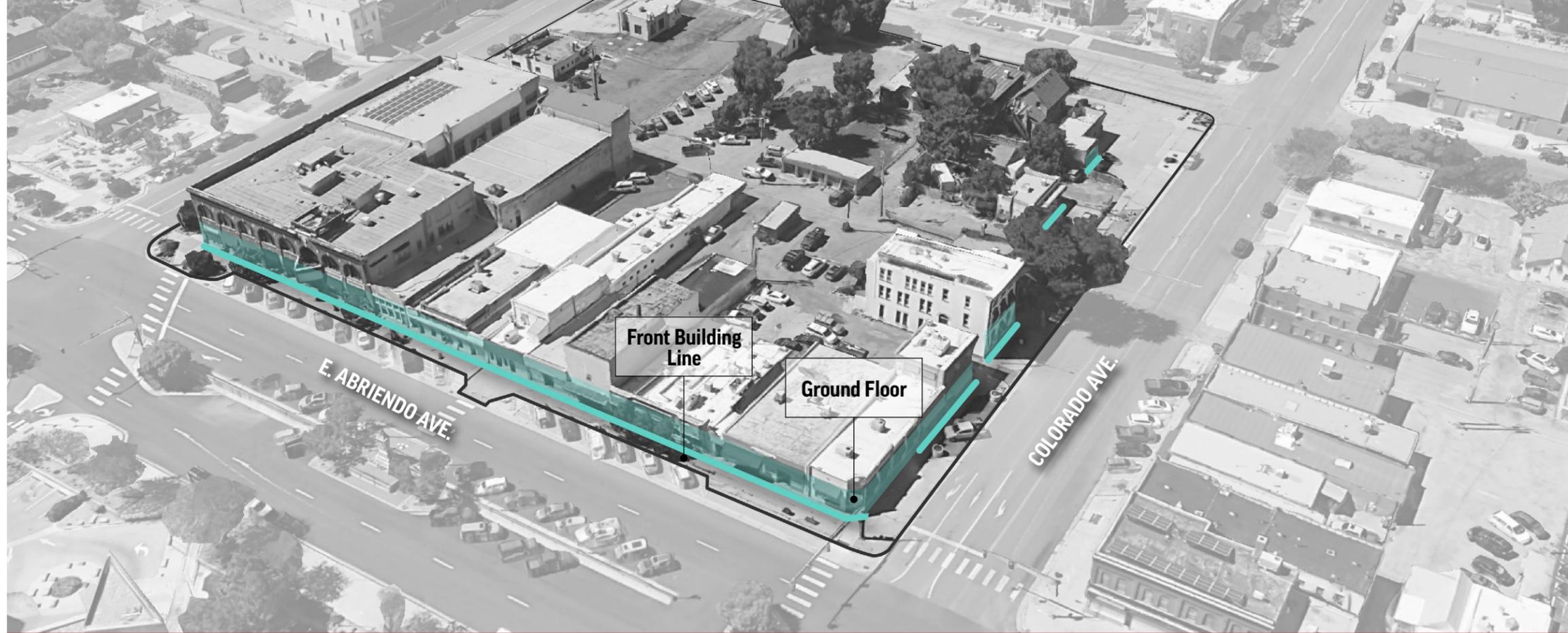
A series of diagrams shows a closer look at a portion of the example block shown in plan view on the previous page.

-  Ground Floor
-  Front Building Line

The side facing E. Abriendo Ave. has a well-established frontage to connect to the sidewalks.

-  Streetscape Amenity Zone

Limited streetscape amenities are available in the non-residential context. The residential context has a wide landscape zone.



Typical Urban Design Elements

▬▬▬▬▬ Sidewalk
 Curb Cut

The example block has continuous sidewalks on the side facing E. Abriendo Ave. but suffers from frequent interruptions from curb cuts on the other side of the block.



▬▬▬▬▬ Access (Driveway & Parking)
 Curb Cut

The block's alley helps to reduce the need for front driveways, enhancing the pedestrian experience along E. Abriendo Ave. However, the other half of the block has several private driveways, and parking areas fronting on to the sidewalks.



Building Types

The Mesa Junction area has a variety of scales and types of commercial and mixed-use building. Building and frontage designs are varied even in one block, changing from a pedestrian-oriented commercial area to an auto-oriented commercial area down the street.



Mixed Use Building

Scale: Small | Building Footprint: approx. 2,605 ft² | Land Use: Commercial
Lot Size: approx. 2,605 ft² | Height: 2 stories | Frontage Type: Street Front



Mixed Use Building

Scale: Large | Building Footprint: approx. 24,086 ft² | Land Use: Commercial
Lot Size: approx. 25,334 ft² | Height: 2 stories | Frontage Type: Street Front



Commercial

Scale: Small | Building Footprint: approx. 1,015 ft² | Land Use: Commercial
Lot Size: approx. 4,697 ft² | Height: 1 story | Frontage Type: Parking



Library

Scale: Large | Building Footprint: approx. 38,962 ft² | Land Use: Institutional
Lot Size: approx. 175,294 ft² | Height: 3 stories | Frontage Type: Civic Space



Commercial

Scale: Small | Building Footprint: approx. 1,608 ft² | Land Use: Commercial
Lot Size: approx. 1,608 ft² | Height: 1 story | Frontage Type: Street-Front



Commercial

Scale: Small | Building Footprint: approx. 1,273 ft² | Land Use: Commercial
Lot Size: approx. 13,915 ft² | Height: 1 story | Frontage Type: Civic Space



Commercial

Scale: Small | Building Footprint: approx. 1,039 ft² | Land Use: Commercial
Lot Size: approx. 9,052 ft² | Height: 1 story | Frontage Type: Parking & Driveway



Museum

Scale: Small | Building Footprint: approx. 1,650 ft² | Land Use: Commercial
Lot Size: approx. 2,185 ft² | Height: 2 stories | Frontage Type: Street-Front

Key Takeaways

Block Pattern

- The gridded block pattern and small to moderate block sizes of the Grid development pattern creates highly connected and walkable environments.



Open Space

- A variety of open space typologies embedded within neighborhoods and mixed use areas creates gathering places and amenities for nearby residents, while providing ecosystem services like shade and stormwater management.



Streets

- A wide streetscape amenity zone is common in Grid neighborhoods, providing space for street trees and buffering sidewalks from adjacent traffic. Wide neighborhood streets provide plenty of space for on-street parking.



Urban Design Elements & Frontage

- Alley access is typical, creating a safe and comfortable environment for pedestrians and bicyclists. Frontage elements create a strong relationship between private development and the public realm.



Building Types

- Grid neighborhoods feature a variety of housing types and scales, providing a wide range of housing options for residents. In many areas, historic buildings can still be seen and provide opportunity for adaptive reuse.



Downtown Pueblo



Belmont Neighborhood, Pueblo

2 TRANSITION

Introduction

The Transition development pattern is typically found where a grid pattern starts to morph into a more curvilinear “suburban” pattern, marking the transition between different formats of development. This kind of “transitional” street pattern typically supports moderately connected neighborhoods and commercial centers.

In residential areas, block sizes are small to moderate and alleys are uncommon. Residential streets typically have a continuous sidewalk network and may also include bike lanes, but the lack of alleys leads to frequent breaks in the sidewalk for private driveways.

Non-residential block sizes are large and irregular, with frequent curb cuts interrupting sidewalk continuity. While the sidewalk network remains continuous, bike lanes are rare, and streets within the Transition development pattern typically shows a greater orientation towards automobile transportation over other modes of transportation.

Overall, the relationship between the public realm and private development begins to break down in Transition areas, and barriers between residential and non-residential areas can be seen forming.



Residential Context 01.

Belmont Neighborhood

Mixed Housing Neighborhood

--- Development Pattern Example Area

The Belmont Neighborhood is a typical example of the residential Transition development pattern in Pueblo.



Block Pattern

Transition Block Pattern

Block Size: varies, approx. 2.6 acres - 25 acres

Block Dimensions: varies, approx. 600' x 220' - 1,840' x 800'

- Street Network
- Trails
- - - Example Boundary
- Parks & Open Space

Transition pattern blocks greatly vary in size, ranging from a little under three acres to twenty-five acres in the example area illustrated to the right. The block pattern does not follow a rectilinear gridded pattern like the Grid Development Pattern, but maintains greater connectivity between blocks than the Suburb Development Pattern, following a “modified grid” pattern. Culs-de-sac begin to emerge in the Transition pattern, and connectivity between residential areas and non-residential areas begins to diminish.



Typical Open Space Typologies

Transition neighborhoods typically have a variety of park and open space types within and adjacent to them. Park typologies found in transition neighborhoods can range from smaller School Parks that serve the surrounding neighborhoods, to large Regional Parks and Open Space corridors that serve as destinations and ecological resources for the Pueblo region.



Belmont Park
Park Type: Regional Park
Size: 10 acres



Haff Park
Park Type: School Park
Size: 10 acres



Fountain Creek Trail Corridor
Park Type: Open Space (Trail)
Size: 430 acres

Typical Street Typologies

Streets in the Belmont neighborhood represent several functional classes, from minor arterial streets, to collector streets, to local streets. The streetscape amenity zone begins to appear less frequently in this pattern, and where present, tends to include few amenities besides street trees. While sidewalk widths are narrower in Transition neighborhoods, sidewalks are

consistently present. Although many streets do provide space for on-street parking, it is often underutilized in favor of parking in private yards or in the streetscape amenity zone.

Note: Roadway widths are measured from curb edge to curb edge.



Sidewalk	Parking	Bike	Travel Lane	Travel Lane	Bike	Parking	Sidewalk
4'	7'	5'	9'	12'	5'	7'	4.5'
Roadway 37'							

Bonforte Blvd.
Functional Class: Minor Arterial
Roadway Width: 45'
Multimodal Feature(s): Bike Lane, Buffered Sidewalks



Sidewalk	Buffer	Parking	Travel Lane	Travel Lane	Parking	Buffer	Sidewalk
4.5'	5'	7'	8'	8'	7'	5'	4.5'
Roadway 30'							

Kickapoo Rd.
Functional Class: Local
Roadway Width: 30'
Multimodal Feature(s): Buffered Sidewalks



Sidewalk	Buffer	Parking	Travel Lane	Travel Lane	Parking	Buffer	Sidewalk
4.5'	5'	7'	8'	8'	7'	5'	4.5'
Roadway 30'							

Horseshoe Dr.
Functional Class: Collector
Roadway Width: 30'
Multimodal Feature(s): Buffered Sidewalks

Street Design Elements

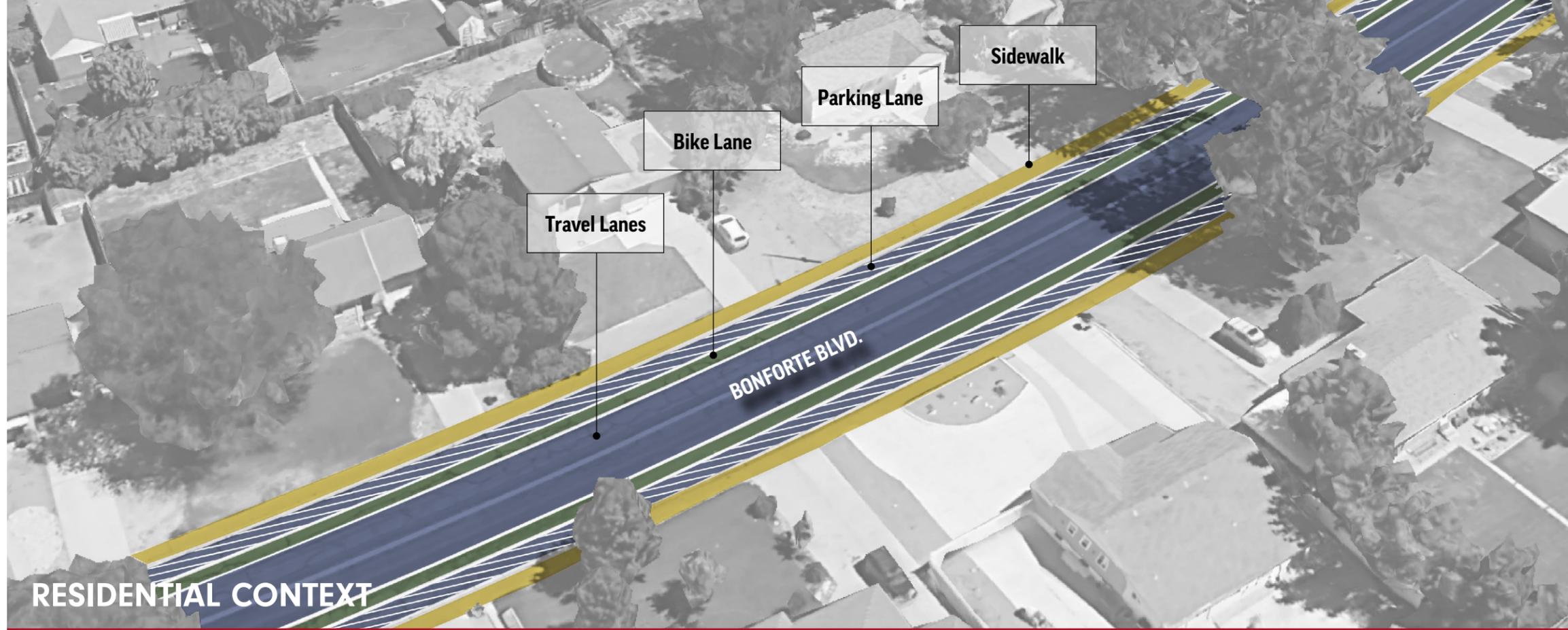
Street Design Elements

- Sidewalk
- Bike Lane
- Streetscape Amenity Zone
- Parking
- Travel Lane

Street Design & Transition

Bonforte Blvd. is one example of a street in this pattern that transitions from a residential context to a non-residential context. In its residential context, Bonforte Boulevard includes bike lanes, on-street parking, and protected sidewalks. However, as the street moves into non-residential areas, the street design alters to favor vehicular transportation: the bike lanes are discontinued and sidewalks are no longer protected.

Context	Residential	Non-residential
Roadway Width	45'	75'
Roadway	Bike lanes and parking lanes on both sides of street.	N/A
Streetscape Elements	N/A	Median



Typical Urban Design Elements

Urban Design Elements

- Front Building Line
- Sidewalk
- Access (Driveways & Parking)
- Streetscape Amenity Zone

In Transition neighborhoods, a consistent pattern of front building lines is still preserved. Sidewalks are also consistently present, although the lack of alleys in favor of front-loaded driveways means that the sidewalk is frequently interrupted by driveways. Driveways lead to front- or side-attached garages. The streetscape amenity zone is inconsistently present, and could be improved by increasing street lights, shade trees, and other amenities.



Example Block Location

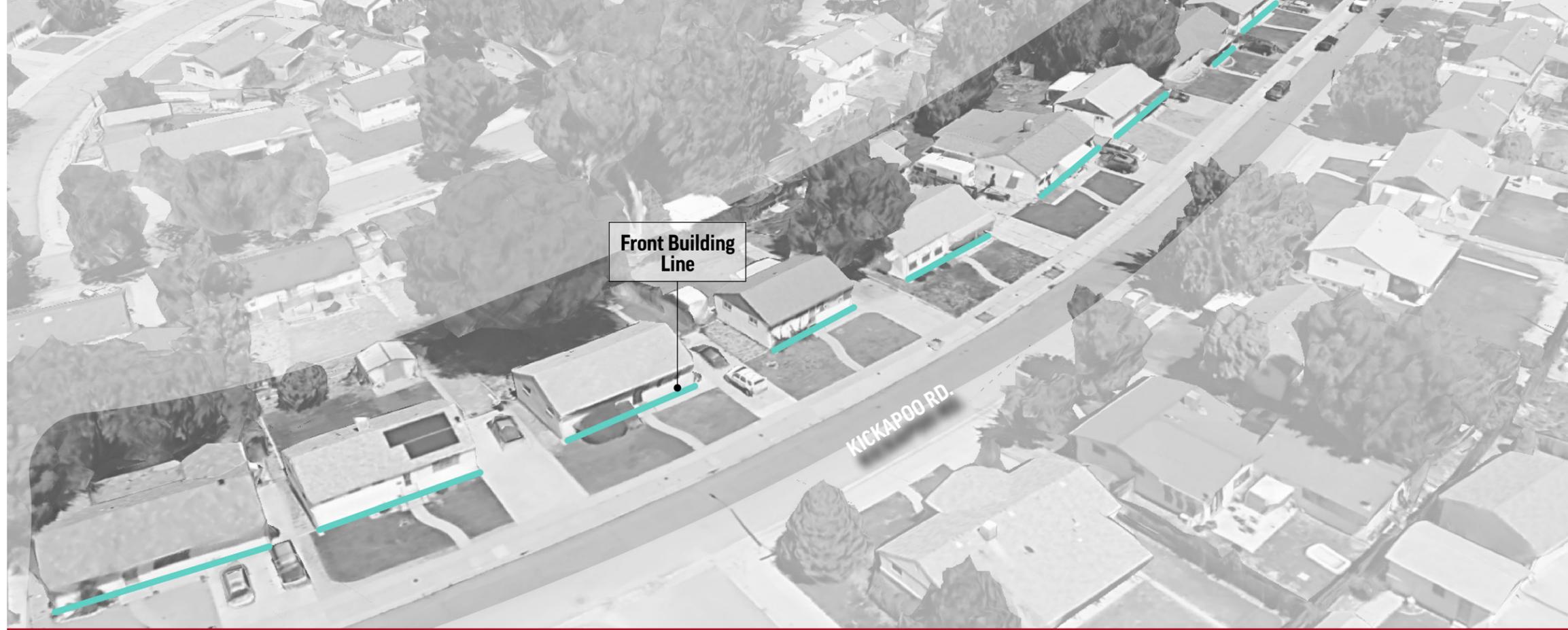


Typical Urban Design Elements

This series of diagrams takes a closer look at a portion of the Transition example block (previously shown in plan view).

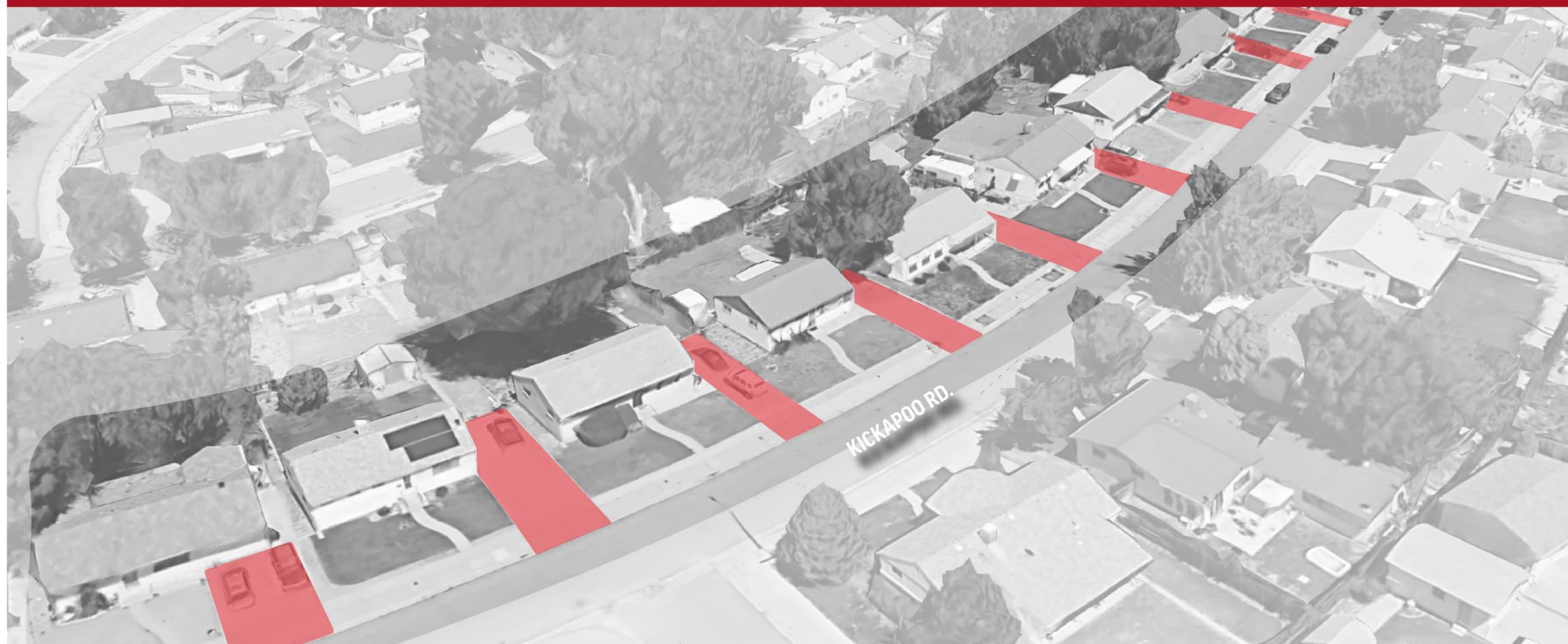
Front Building Line

The example block has a consistent pattern of front building lines, moderate setbacks, and walks connecting each home to the sidewalk network. These features contribute to a well-established frontage relationship between public and private space.



Access (Driveway & Parking)

The block's pattern of front and side driveways causes frequent interruptions to the sidewalk and places pedestrians and cars in closer proximity to each other, negatively impacting the safety and comfort of active transportation.

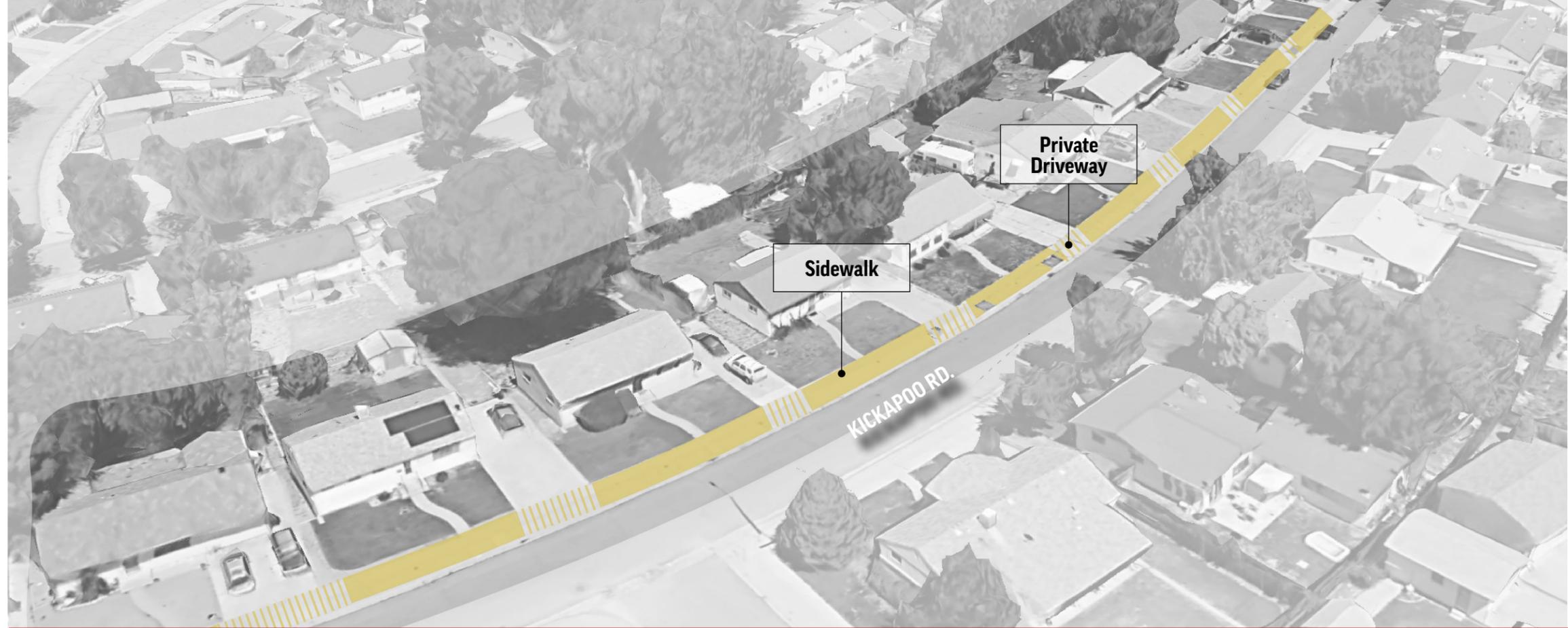


Typical Urban Design Elements

This series of diagrams takes a closer look at a portion of the Transition example block (previously shown in plan view).

Sidewalk

The example block has continuous sidewalks, but there are frequent breaks (indicated by the dashed lines) in the sidewalk for private driveways.



Streetscape Amenity Zone

The example block has almost no streetscape amenity zone to provide shade trees, street lights, green infrastructure, and other streetscape amenities.



Frontage Types

The frontage types common in Transition neighborhoods exhibit less variety than in Grid neighborhoods. Entry features are typically recessed or stoop entries, and porches are rare. Driveways are a prominent feature of residential frontage with some homes featuring multiple driveways -- negatively impacting the pedestrian experience. Parking typically occurs

in front- or side-attached garages, although some homes still have rear detached garages access by side driveways. The combination of these features begin to show a breakdown of relationship between private development and the public realm.

Frontage & Access

- Entry Features
- Access (Driveways & Parking)



Entry Features: Stoop | Access Format: Side Driveway
Parking Format: Side Driveway



Entry Features: Recessed | Access Format: Front & Side Driveways
Parking Format: Side Attached & Rear Detached Garages, Car-Port



Entry Features: Recessed Stoop | Access Format: Front Driveway
Parking Format: Front Attached Garage



Entry Features: Recessed & Porch | Access Format: Front Driveway
Parking Format: Front Attached Garage



Entry Features: Recessed Stoop | Access Format: Side Driveways
Parking Format: Rear Detached Garage



Entry Features: Stoop | Access Format: Side Driveways
Parking Format: Rear Detached & Side Attached Garages

Building Types

Transition neighborhoods still show a variety of housing types. Detached homes, duplexes, multi-unit houses, townhouses, and medium-sized apartment buildings are common. This variety of single-family and missing-middle housing types are typically small or medium in scale, and range from one story to three stories in height.



Detached House

Scale: Small | Building Footprint: approx. 1,115 ft²
Lot Size: approx. 6,140 ft² | Height: 1 stories | Frontage Type: Neighborhood Yard



Detached House

Scale: Medium | Building Footprint: approx. 2,720 ft²
Lot Size: approx. 8,660 ft² | Height: 1 story | Frontage Type: Suburban Yard



Townhouse

Scale: Medium | Building Footprint: approx. 7,970 ft²
Lot Size: approx. 23,110 ft² | Height: 1 story | Frontage Type: Parking



Duplex

Scale: Small | Building Footprint: approx. 2,900 ft²
Lot Size: approx. 8,630 ft² | Height: 1 story | Frontage Type: Suburban Yard



Multi-Unit House (4 Units)

Scale: Small | Building Footprint: approx. 2,650 ft²
Lot Size: approx. 11,900 ft² | Height: 2 stories | Frontage Type: Parking



Apartment Building

Scale: Medium | Building Footprint: 25,000 ft²
Lot Size: 288,100 ft² | Height: 3 stories | Frontage Type: Buffer



Non-Residential Context 01.

Belmont & East Side Neighborhoods

Commercial Center

--- Development Pattern Example Area

This commercial center is located where the Belmont neighborhood meets the East Side neighborhood, and is a typical example of the Transition development pattern in Pueblo.



Block Pattern

Transition Block Pattern

Block Size: varies, approx. 11 acres - 30 acres

Block Dimensions: varies, approx. 530' x 1,155' - 1,340' x 2,200'

- Street Network
- Trails
- Example Boundary
- Parks & Open Space
- ||||| Rail

Non-residential Transition pattern blocks greatly vary in size, ranging from a little under three acres to twenty-five acres in the example area illustrated to the right. The block pattern does not follow a rectilinear pattern like the Grid Development Pattern, but maintains more connectivity between blocks and between residential and non-residential areas than the Suburb Development Pattern. A pattern of large-scale commercial sites with frontages dominated by surface parking begins to emerge in the Transition pattern.



Typical Open Space Typologies

Transition non-residential areas share the same variety of open spaces types as transition residential areas. Park typologies found in transition neighborhoods can range from smaller School Parks that serve the surrounding neighborhoods, to large Regional Parks and Open Space corridors that serve as destinations and ecological resources for the Pueblo region.



Belmont Park
Park Type: Regional Park
Size: 10 acres



Haff Park
Park Type: School Park
Size: 10 acres



Fountain Creek Trail Corridor
Park Type: Open Space (Trail)
Size: 430 acres

Typical Street Typologies

Street types in non-residential Transition areas include minor arterial streets and local streets. The streetscape amenity zone is rarely present. While sidewalks are consistently present, they have no protection or buffering from adjacent traffic and are typically narrow -- negatively impacting pedestrian safety.

Overall, these street typologies favor automobile transportation over all other modes.

Note: Roadway widths are measured from curb edge to curb edge.



Sidewalk	Travel Lane	Travel Lane	Median	Travel Lane	Travel Lane	Sidewalk
6'	16'	12'	15.5'	11.5'	12'	4.5'
Roadway						
67'						

Bonforte Boulevard
Functional Class: Minor Arterial
Roadway Width: 67'
Multimodal Feature(s): N/A



Sidewalk	Travel Lane	Center Turn Lane	Travel Lane	Turn Lane	Sidewalk
6'	15'	10'	11'	11'	6.5'
Roadway					
47'					

Hunter Drive
Functional Class: Local
Roadway Width: 47'
Multimodal Feature(s): N/A



Sidewalk	Travel Lane	Travel Lane	Parking Lane	Sidewalk
4'	13.5'	12.5'	9'	4'
Roadway				
35'				

Ruppel Street
Functional Class: Local
Roadway Width: 35'
Multimodal Feature(s): N/A

Street Design Elements

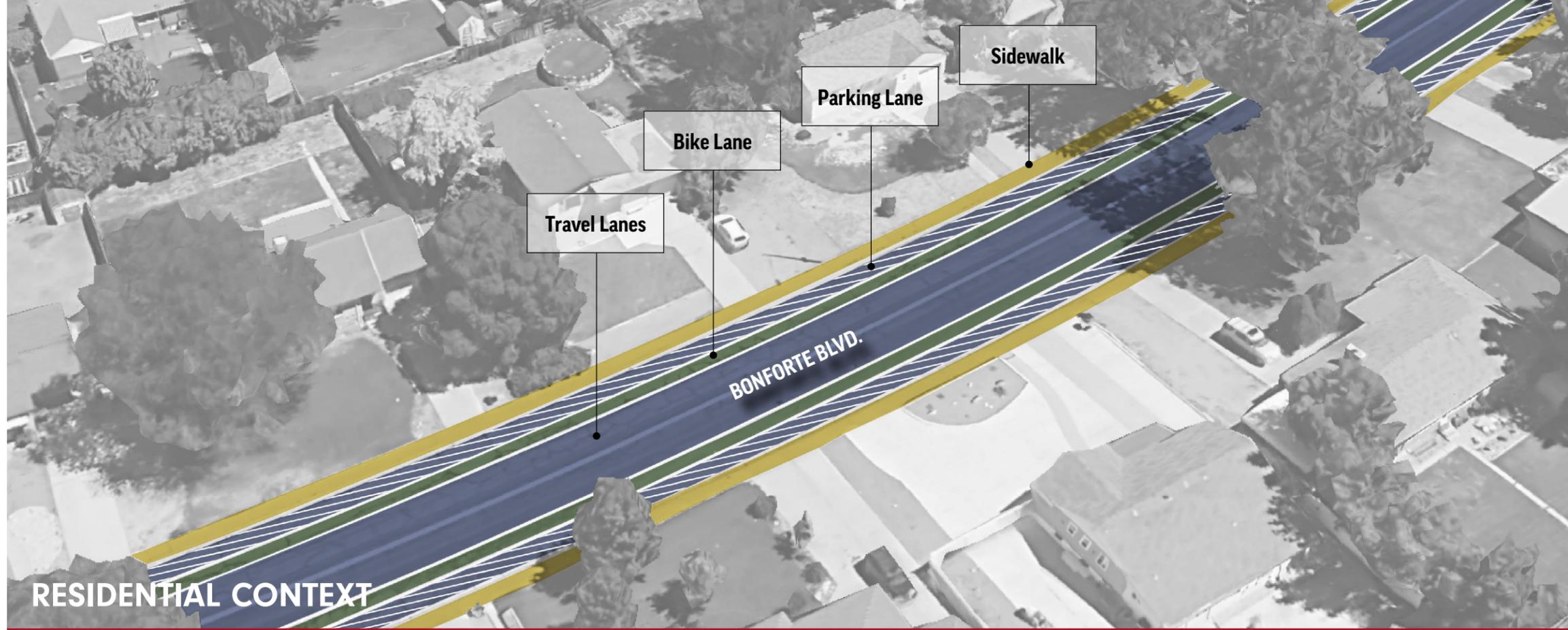
Street Design Elements

- Sidewalk
- Bike Lane
- Streetscape Amenity Zone
- Parking
- Travel Lane

Street Design & Transition

Bonforte Blvd. is one example of a street in this pattern that transitions from a residential context to a non-residential context. In its residential context, Bonforte Boulevard includes bike lanes, on-street parking, and protected sidewalks. However, as the street moves into non-residential areas, the street design alters to favor vehicular transportation: the bike lanes are discontinued and sidewalks are no longer protected.

Context	Residential	Non-residential
Roadway Width	45'	75'
Roadway	Bike lanes and parking lanes on both sides of street.	N/A
Streetscape Elements	N/A	Median



Typical Urban Design Elements

Urban Design Elements

- Front Building Line
- Sidewalk
- Access (Driveways & Parking)
- Streetscape Amenity Zone
- Curb Cut

In Transition non-residential areas, there is no consistent pattern of frontage: front building lines are rarely oriented towards the street, surface parking and landscape buffers dominate frontages, and while the sidewalk is continuous it is frequently interrupted by curb cuts. The streetscape amenity zone is rare and lacks any amenities.



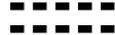
Example Block Location



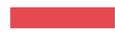
Typical Urban Design Elements

This series of diagrams takes a closer look at a portion of the Transition example block (previously shown in plan view).

 Sidewalk

 Curb Cut

The example block has consistent sidewalk presence, but there are frequent breaks (indicated by the dashed lines) for curb cuts.

 Access (Driveway & Parking)

 Curb Cut

Access and parking is the dominant feature of non-residential Transition sites. Large surface parking lots dominate the space between buildings and the street.



Typical Urban Design Elements

This series of diagrams takes a closer look at a portion of the Transition example block (previously shown in plan view).

- Ground Floor
- Front Building Line

Large setbacks, inconsistent front building line patterns, and minimal ground floor transparency create no relationship between the street and private development.

- Buffer Space

There is no streetscape amenity zone, but large landscape buffers further separate private development from the street.



Building Types

Non-residential Transition areas include a variety of building types oriented towards providing a regional commercial center. Retail of all scales, drive-thrus, gas stations, medical facilities, hotels, and institutional services like libraries are common. Larger-scale commercial buildings are typically located towards the rear or centers of sites, with smaller-scale pad sites or drive-thrus located towards the front of sites.



Retail Strip

Scale: Large | Building Footprint: approx. 93,500 ft² | Land Use: Commercial
Lot Size: approx. 354,080 ft² | Height: 1 stories | Frontage Type: Parking



Box Store

Scale: Medium | Building Footprint: approx. 16,540 ft² | Land Use: Commercial
Lot Size: approx. 118,430 ft² | Height: 1 story | Frontage Type: Parking & Buffer



Gas Station & Convenience Store

Scale: Small | Building Footprint: approx. 3,300 ft² | Land Use: Commercial
Lot Size: approx. 34,760 ft² | Height: 1 stories | Frontage Type: Buffer



Clinic

Scale: Medium | Building Footprint: approx. 15,818 ft² | Land Use: Healthcare
Lot Size: approx. 86,050 ft² | Height: 1 story | Frontage Type: Parking & Buffer



Retail Pad

Scale: Small | Building Footprint: approx. 2,940 ft² | Land Use: Commercial
Lot Size: approx. 20,030 ft² | Height: 1 stories | Frontage Type: Parking



Drive-Thru

Scale: Small | Building Footprint: approx. 4,470 ft² | Land Use: Commercial
Lot Size: approx. 4,0540 ft² | Height: 1 story | Frontage Type: Buffer & Parking



Motel

Scale: Medium | Building Footprint: approx. 23,453 ft² | Land Use: Commercial
Lot Size: 219,000 ft² | Height: 2 stories | Frontage Type: Buffer



Library

Scale: Medium | Building Footprint: 8,030 ft² | Land Use: Institutional
Lot Size: 353,570 ft² | Height: 1 stories | Frontage Type: Buffer

Key Takeaways

Block Pattern

Although block sizes are typically larger in the Transition pattern, both residential and non-residential areas are still moderately well connected.



Open Space

The Transition pattern retains a variety of different types and scales of open space, ranging from neighborhood parks to larger open space and trail corridors.



Streets

Streets and streetscape elements vary widely in this pattern. Residential streets tend to preserve better walkability and bike infrastructure, while non-residential streets tend to favor vehicular transportation over other modes.



Urban Design Elements & Frontage

Alleys are not found in this pattern, and private driveways are the most prominent frontage features. Sidewalks remain consistently present, while the streetscape amenity zone is less consistent. In this pattern, a breakdown in the relationship between private development and the public realm can be seen.



Building Types

Transition neighborhoods feature a wide variety of housing types integrated into the neighborhoods. Detached homes, duplexes, tri- and quad-plexes exist cohesively together. Apartments are scattered throughout, typically on the edges of residential areas. Non-residential buildings vary widely in scale and are clustered into commercial centers or corridors.



Belmont Neighborhood, Pueblo



Ridge Neighborhood, Pueblo

3 SUBURB

Introduction

The Suburb development pattern is typically found outside of Transition pattern areas, in parts of the city developed in more recent decades. The pattern is typically fully curvilinear, and residential areas are characterized by many culs-de-sac and internally-oriented neighborhood developments. Non-residential areas are characterized by very large developments with large-scale commercial sites in the center of a block and surrounded by small pad sites.

In both residential and non-residential areas, block sizes can vary greatly in size and shape, and do not form identifiable

patterns. Streets typically include narrow sidewalks, rarely include a streetscape amenity zone, but may sometimes include bike lanes. In both residential and non-residential areas, frequent curb cuts and private driveways interrupt both sidewalks and bike lanes (if present).

Overall, the relationship between the public realm and private development in Suburb pattern areas lacks connection. Residential and non-residential areas are not typically integrated together, and transportation facilities are heavily skewed to favor vehicular transportation over all other modes.



Residential Context 01.

University Neighborhood

Detached Housing

--- Development Pattern Example Area

The University Neighborhood is a typical example of a residential Suburb development pattern in Pueblo, composed entirely of detached homes.

Note: This neighborhood does not share a street with a non-residential area, so a Street Design Elements page is not included in this context.



Block Pattern

Suburb Block Pattern

Block Size: varies, approx. 2.7 acres - 14 acres

Block Dimensions: varies, approx. 306' x 370' - 1,320' x 572'

- Street Network
- Trails
- - - Example Boundary
- Parks & Open Space

Suburb pattern blocks vary in size, ranging from almost three acres to approximately fourteen acres in the example area illustrated to the right. The block pattern is fully curvilinear, following a traditionally "suburban" pattern. Culs-de-sac are a prominent feature of Suburb neighborhoods. Connectivity between residential and non-residential areas is limited and indirect, creating challenges for non-vehicular transportation modes.



Typical Open Space Typologies

Suburb neighborhoods have a variety of park and open space types. Park typologies found near the University neighborhood range from small neighborhood parks serving the immediate neighborhood, to large open space corridors, to specialized facilities (like public golf courses).



University Park
Park Type: Neighborhood Park
Size: 16 acres



Fountain Creek Trail
Park Type: Open Space & Trail
Size: 430 acres



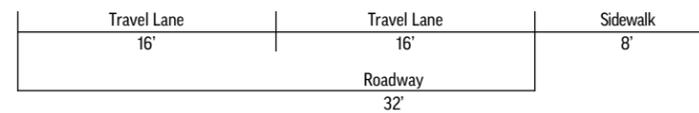
Walking Stick Public Golf Course
Park Type: Specialized Facility
Size: 179.5 acres

Typical Street Typologies

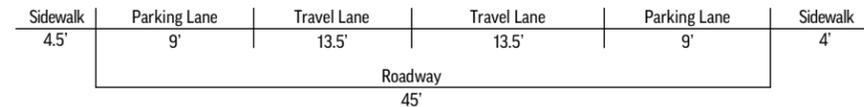
Streets in the University neighborhood represent several functional classes, from minor arterial streets, to collector streets, to local streets. The streetscape amenity zone is rarely found in this pattern, sidewalk widths are noticeably narrow, and bike lanes rare. The majority of the right-of-way space is

dedicated to vehicular use through travel lanes and parking lanes.

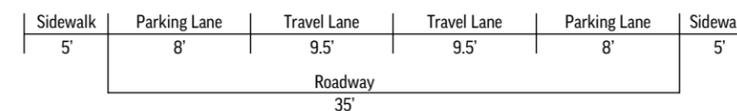
Note: Roadway widths are measured from curb edge to curb



47th Street
Functional Class: Minor Arterial
Roadway Width: 32'
Multimodal Feature(s): Wide sidewalk



Cedarweed Boulevard
Functional Class: Collector
Roadway Width: 45'
Multimodal Feature(s): N/A



Ironweed Drive
Functional Class: Local
Roadway Width: 35'
Multimodal Feature(s): N/A

Typical Urban Design Elements

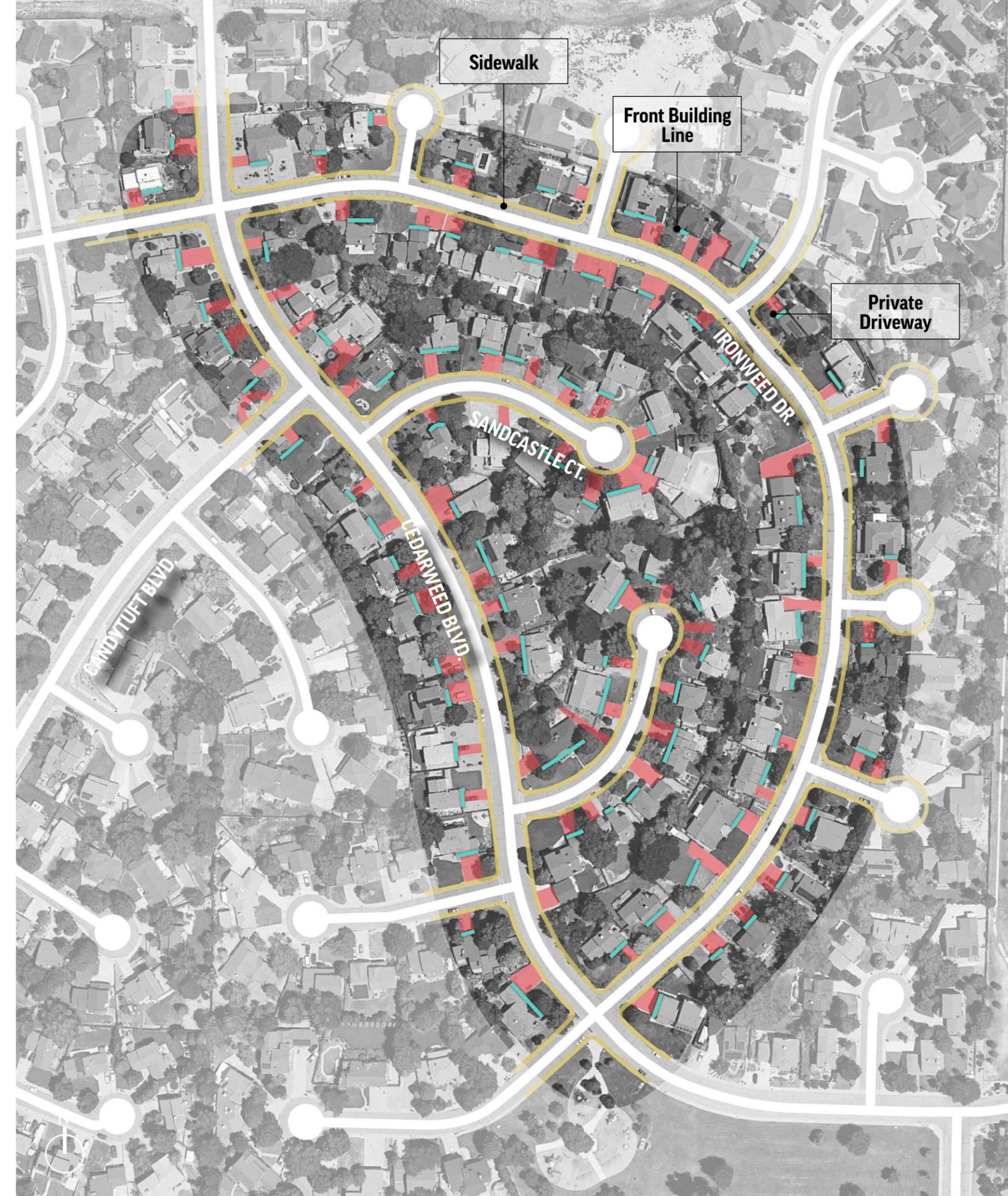
Urban Design Elements

- Front Building Line
- Sidewalk
- Access (Driveways & Parking)
- Streetscape Amenity Zone

A pattern of front building lines is largely preserved in Suburb neighborhoods, although it is less consistent than Grid or Transition neighborhoods. Sidewalks are also consistently present, but are frequently interrupted by private driveways that lead to front- or side-attached garages. The streetscape amenity zone is not present.



Example Block Location



Typical Urban Design Elements

This series of diagrams takes a closer look at a portion of the example block (shown in plan view on the previous page).

Note: This neighborhood does not contain a streetscape amenity zone, so no streetscape amenity zone diagram is shown.

Front Building Line

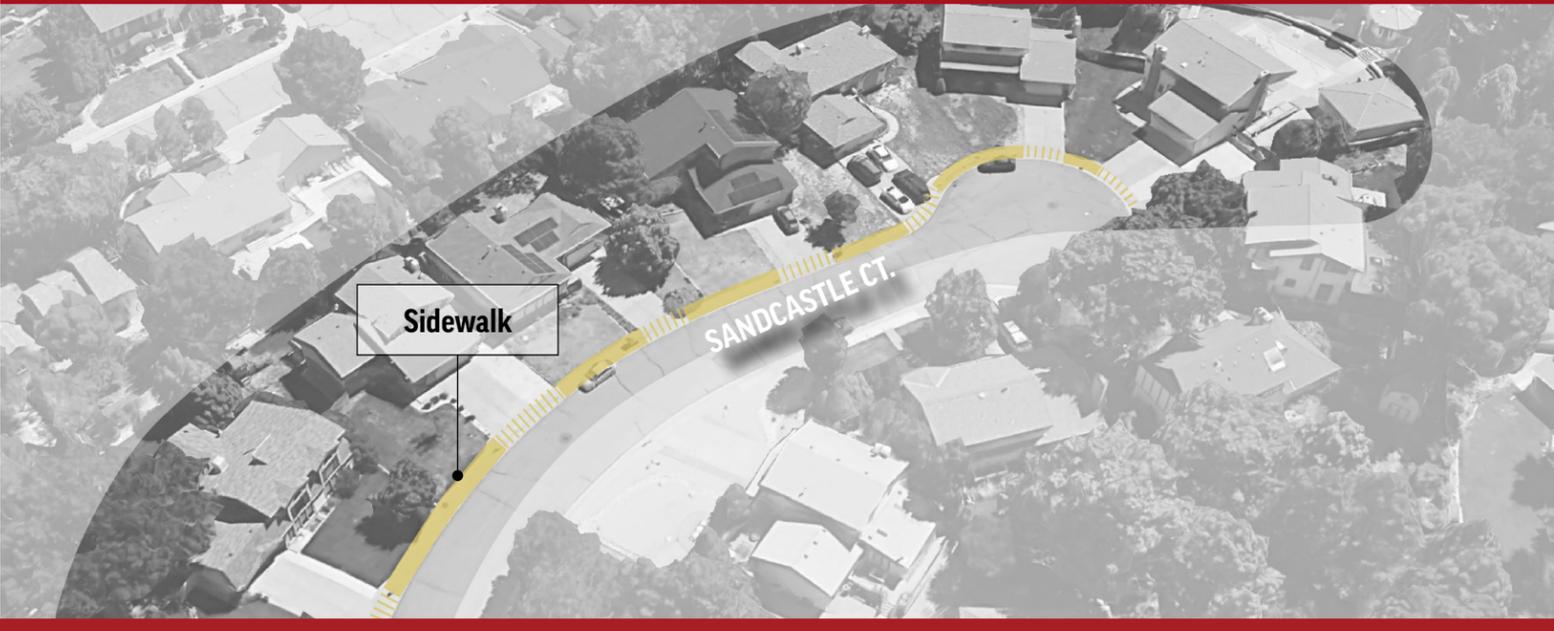
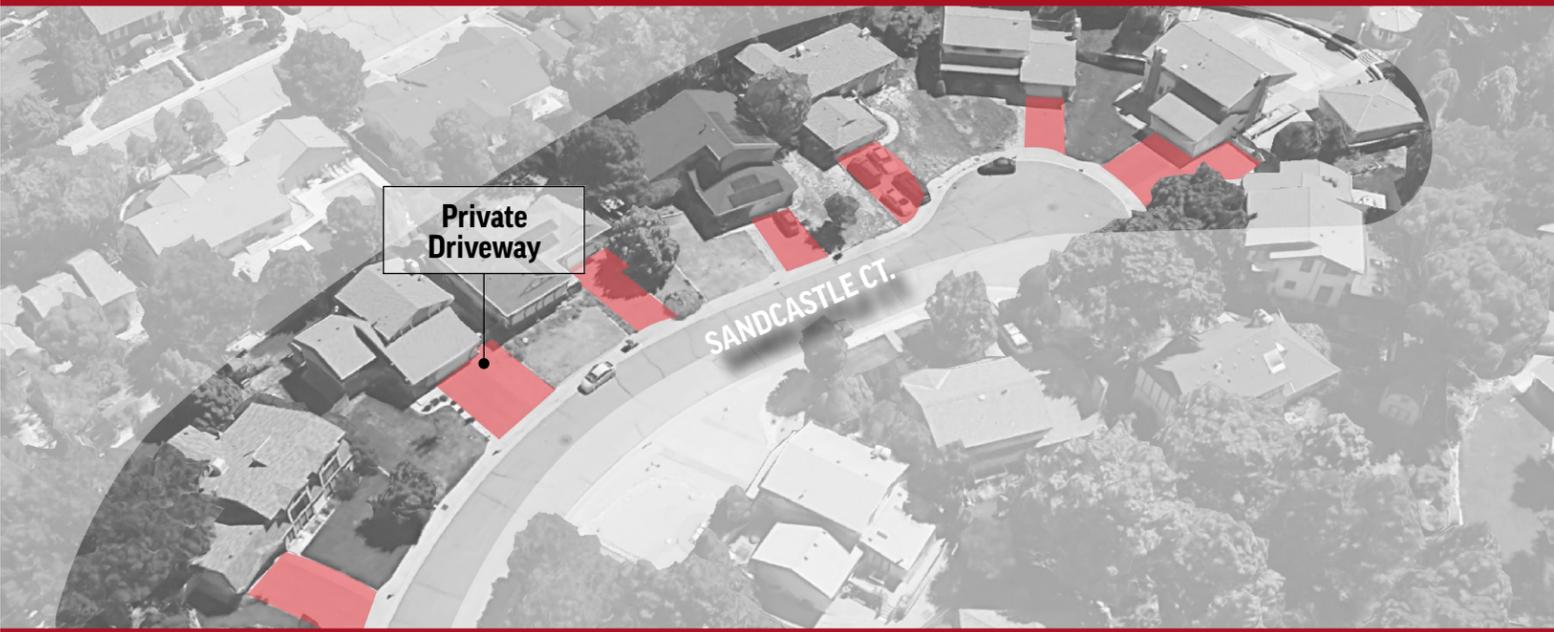
The example block has a somewhat consistent pattern of front building lines and setbacks.

Access (Driveway & Parking)

The block's pattern of front driveways causes frequent interruptions to the sidewalk and places pedestrians and cars in closer proximity to each other, negatively impacting the safety and comfort of active transportation.

Sidewalk

The example block has consistent sidewalk presence, but there are frequent breaks (indicated by the dashed lines) in the sidewalk for private driveways.



Frontage Types

The frontage types common in Transition neighborhoods exhibit little variety: entries are typically recessed or stoop entries, and porches are rare. Driveways are a prominent feature and Parking typically occurs in front- or side-attached garages. The only relationship between private development and the public realm is vehicle-oriented.

Frontage & Access

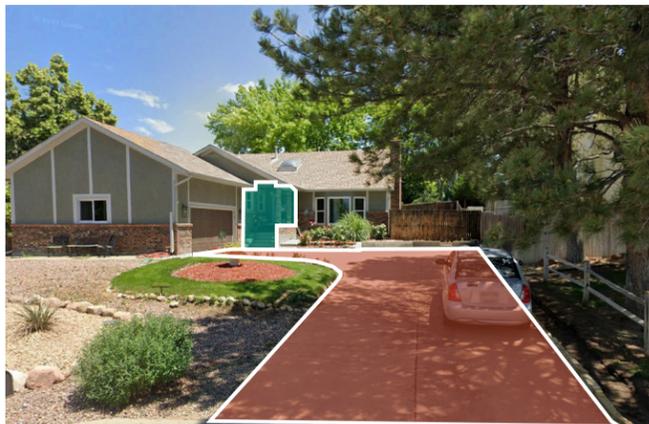
- Entry Features
- Access (Driveways & Parking)



Entry Features: Recessed Stoop | Access Format: Front Driveway
Parking Format: Front Attached Garage



Entry Features: Recessed Porch | Access Format: Front & Side Driveway
Parking Format: Side Attached & Rear Detached Garage



Entry Features: Recessed Stoop | Access Format: Front Driveway
Parking Format: Front Attached Garage



Entry Features: Stoop | Access Format: Front Driveway
Parking Format: Front Attached Garage & Front Parking Yard

Building Types

Suburb neighborhoods still show a lesser variety of housing types, with detached homes being the most common type. The scale typically ranges from small to large, and height ranges from one to two stories.



Detached Single-Family House

Scale: Small | Building Footprint: approx. 1,650 ft²
Lot Size: approx. 8,672 ft² | Height: 1.5 stories | Frontage Type: Suburban Yard



Detached Single-Family House

Scale: Small | Building Footprint: approx. 1,630 ft²
Lot Size: approx. 7,870 ft² | Height: 2 stories | Frontage Type: Suburban Yard



Detached Single-Family House

Scale: Medium | Building Footprint: approx. 2,675 ft²
Lot Size: approx. 15,493 ft² | Height: 2 stories | Frontage Type: Suburban Yard



Detached Single-Family House

Scale: Large | Building Footprint: approx. 4,060 ft²
Lot Size: approx. 16,590 ft² | Height: 1 stories | Frontage Type: Suburban Yard



Residential Context 02.

Ridge Neighborhood

Mixed Housing Suburb

--- Development Pattern Example Area

This example area within the Ridge Neighborhood is a typical example of the residential Suburb development pattern in Pueblo and contains a mix of housing types: detached homes, duplexes, multifamily developments, and senior living developments.



Block Pattern

Suburb Block Pattern

Block Size: varies, approx. 1.8 acres - 31 acres

Block Dimensions: varies, approx. 530' x 200' - 1,320' x 572'

- Street Network
- Trails
- - - Example Boundary

Block sizes in this example exhibit a large difference in scales, ranging from almost two acres to approximately thirty-one acres in the example area illustrated to the right. The block pattern is fully curvilinear, following a traditionally “suburban” pattern. Some developments are only accessible via private streets, and large-scale private development begins to form barriers to connectivity. Connectivity between residential and non-residential areas is limited and indirect, creating challenges for non-vehicular transportation modes.



Typical Open Space Typologies

Suburb neighborhoods have a variety of park and open space types. Park typologies found near the Ridge neighborhood range from activated School Parks serving the immediate neighborhood, to non-activated open spaces.



Eagleridge Open Space

Park Type: Open Space
Size: 3.9 acres



Northridge School Park

Park Type: School Park
Size: 7.7 acres

Typical Street Typologies

Streets in the Ridge neighborhood represent several functional classes, from arterial streets to local streets. The streetscape amenity zone is rarely found in this pattern, sidewalks lack protection from nearby traffic, and bike lanes are rare. The majority of right-of-way space is dedicated to vehicular use

through travel lanes and parking lanes.

Note: Roadway widths are measured from curb edge to curb edge.



Travel Lane	Travel Lane	Turn Lane	Median	Travel Lane	Travel Lane	Sidewalk
10'	13'	11'	9.5'	12.5'	9.5'	6'
Roadway						
65.5'						

Eagleridge Boulevard

Functional Class: Major Arterial
Roadway Width: 65'
Multimodal Feature(s): Buffered sidewalk



Sidewalk	Parking Lane	Travel Lane	Travel Lane	Parking Lane	Sidewalk
5.5'	8'	9'	9'	8'	5.5'
Roadway					
34'					

Pioneer Road

Functional Class: Local
Roadway Width: 34'
Multimodal Feature(s): N/A



Travel Lane	Travel Lane	Sidewalk
15'	15'	6.5'
Roadway		
30'		

Buchanan Drive

Functional Class: Local
Roadway Width: 30'
Multimodal Feature(s): N/A

Street Design Elements

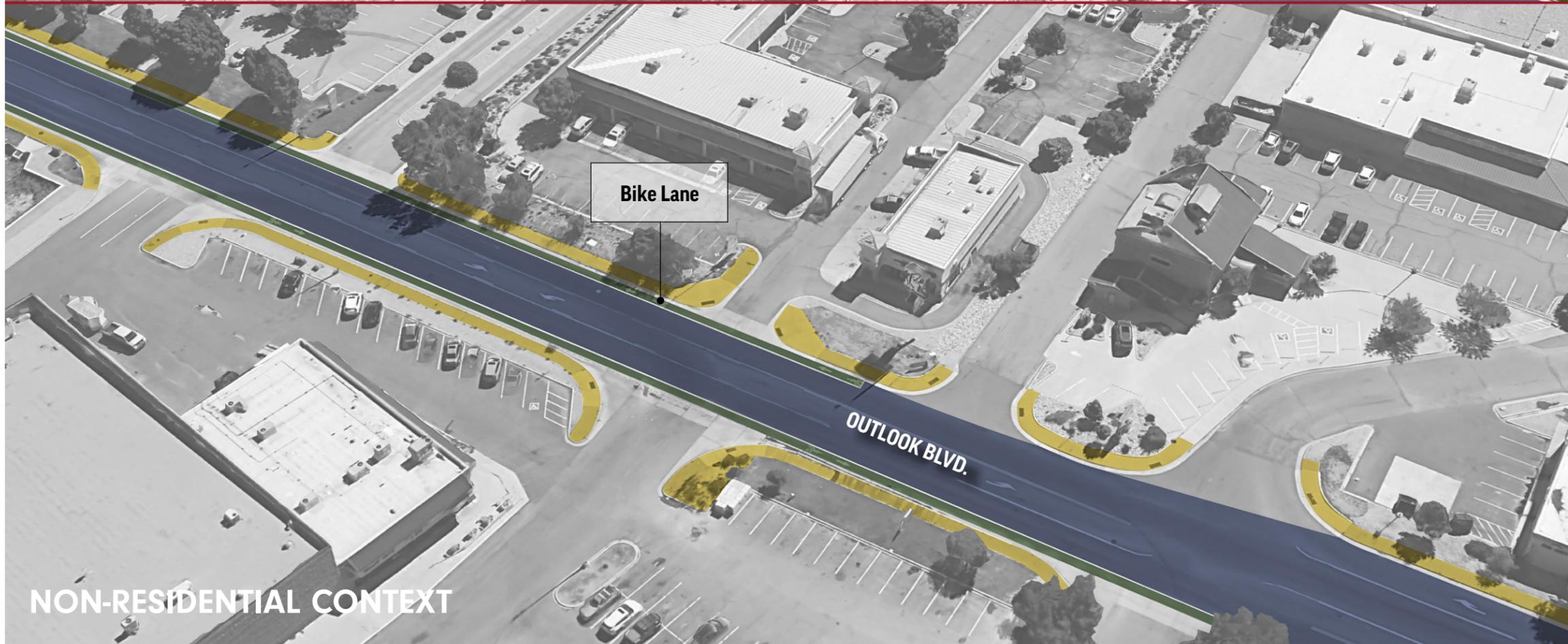
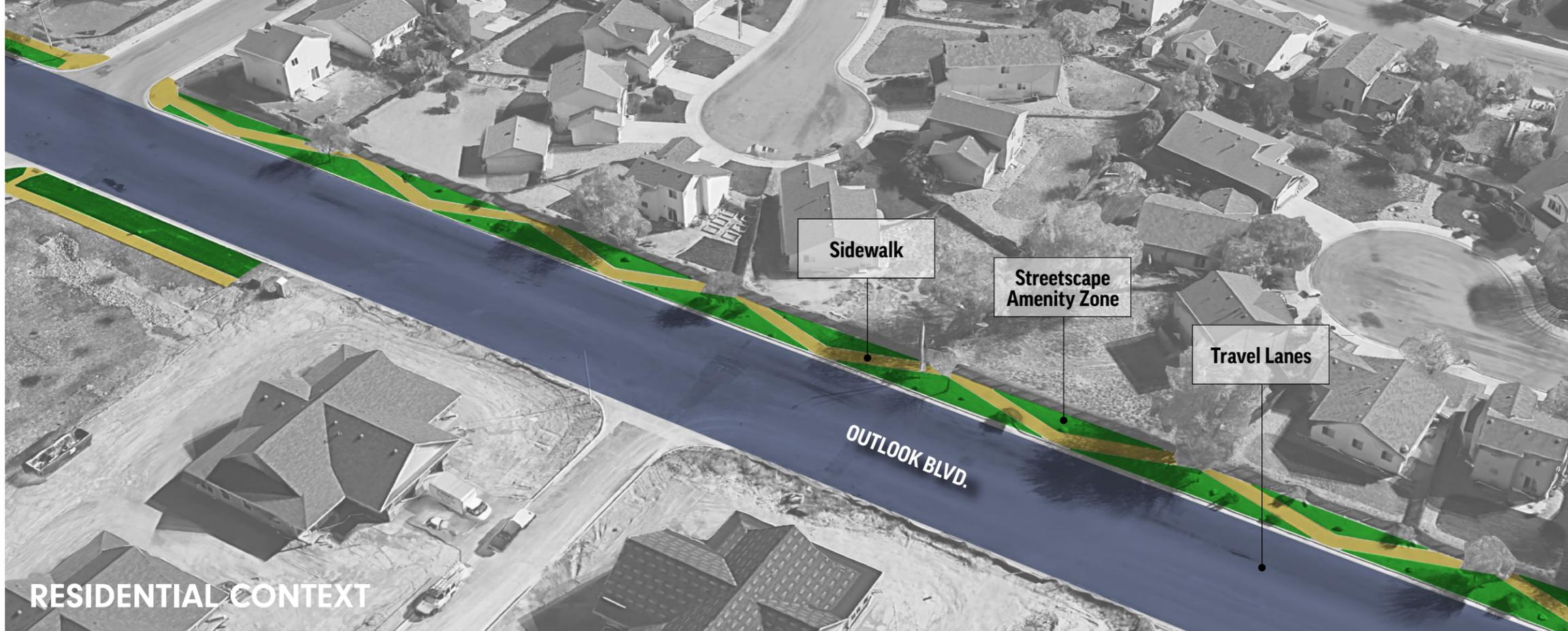
Street Design Elements

-  Sidewalk
-  Bike Lane
-  Streetscape Amenity Zone
-  Parking
-  Travel Lane

Street Design & Transition

Outlook Boulevard is an example of a street in Suburb pattern areas that transitions from a non-residential context to a residential context as it weaves through the Ridge neighborhood. In its residential context, Outlook Boulevard includes a landscaped streetscape amenity zone, protected sidewalks and very wide travel lanes. As Outlook Boulevard street moves into non-residential areas, the street design changes: bike lanes are added, sidewalks remain protected, but there is no streetscape amenity zone.

Context	Residential	Non-residential
Roadway Width	55'	45'
Roadway	N/A	Bike Lanes
Streetscape Elements	Streetscape amenity zone on one side of street; buffered sidewalk.	N/A



Typical Urban Design Elements

Urban Design Elements

- Front Building Line
- Sidewalk
- Access (Driveways & Parking)
- Streetscape Amenity Zone

In the Ridge neighborhood, the pattern of urban design elements differs depending on the surrounding building types. In areas containing detached homes and duplexes, there is a consistent pattern of front building lines and sidewalks, although the lack of alleys in favor of front-loaded driveways means that the sidewalk is frequently interrupted by private driveways leading to front- or side-attached garages. In areas dedicated to multifamily developments, there is no consistent pattern of front building lines facing the street; instead, buildings typically have large setbacks from the street or are oriented towards parking within the site. These developments dedicate a significant amount of space to surface parking. The example area does not contain a streetscape amenity zone, leaving pedestrians without a buffer from passing traffic.



Example Block Location



Typical Urban Design Elements

This series of diagrams takes a closer look at a portion of the example block (shown in plan view on the previous page).

Front Building Line

The example block has a mixture of front building line orientations: detached homes are oriented towards the street, while multifamily buildings are oriented to face internally or, when facing the street, are separated by a large landscape buffer.

Access (Driveway & Parking)

Curbs cuts for private parking access and private driveways interrupt the sidewalk.

Sidewalk

The example block has consistent sidewalk presence, but there are frequent breaks (indicated by the dashed lines) in the sidewalk for private driveways or curb cuts.



Frontage Types

The frontage types common in the Ridge neighborhood differ based on building type. Detached homes exhibit recessed or stoop entries both with and without porches. Driveways are a dominant feature of residential frontage, and parking typically occurs in front- or side-attached garages.



Entry Features: Stoop | Access Format: Front Driveway
Parking Format: Front Attached Garage

Multifamily frontage features large setbacks from the streetscape. Entries are recessed and disconnected from the streetscape by landscaped buffer space or surface parking.



Entry Features: Recessed with Porch | Access Format: Front Driveway
Parking Format: Front Attached Garage

Frontage & Access

- █ Entry Features
- █ Access (Driveways & Parking)



Entry Features: Recessed Stoop | Access Format: Front Driveway
Parking Format: Front Attached Garage



Entry Features: Porch | Access Format: Front Driveway
Parking Format: Front / Side Attached Garage



Entry Features: Stoop | Access Format: Rear Private Drive
Parking Format: Rear Garage & Surface Lot



Entry Features: Porch | Access Format: Private Drive
Parking Format: Surface Parking Lot

Building Types

This neighborhood includes a variety of housing types, though they are generally clustered in separate developments and not integrated together throughout the blocks. Housing types range from small to large detached homes, duplexes, townhomes, to large apartment developments.



Detached House

Scale: Small | Building Footprint: approx. 1,115 ft²
Lot Size: approx. 4,700 ft² | Height: 1 stories | Frontage Type: Suburban Yard



Detached House

Scale: Medium | Building Footprint: approx. 1,660 ft²
Lot Size: approx. 1,662 ft² | Height: 2 stories | Frontage Type: Suburban Yard



Assisted Living Facility

Scale: Large | Building Footprint: approx. 52,138 ft²
Lot Size: approx. 249,562 ft² | Height: 3 stories | Frontage Type: Buffer & Parking



Detached House

Scale: Large | Building Footprint: approx. 2,120 ft²
Lot Size: approx. 5,180 ft² | Height: 2 stories | Frontage Type: Suburban Yard



Duplex

Scale: Medium | Building Footprint: approx. 2,857 ft²
Lot Size: approx. 2,857 ft² | Height: 1 story | Frontage Type: Suburban Yard



Apartment Building

Scale: Large | Building Footprint: approx. 249,562 ft²
Lot Size: approx. 230,000 ft² | Height: 1 stories | Frontage Type: Buffer



Residential Context 03.

Ridge Neighborhood

Mixed Housing Suburb

Development Pattern Example Area

This example area within the Ridge Neighborhood is a typical example of the residential Suburb development pattern in Pueblo and contains a mix of detached homes and duplexes.



Block Pattern

Suburb Block Pattern

Block Size: varies, approx. 3.2 acres - 11.9 acres

Block Dimensions: varies, approx. 270' x 490' - 1,110' x 900'

- Street Network
- - - Example Boundary

Block sizes in this example exhibit a large difference in scales, ranging from a little over three acres to almost twelve acres in the example area illustrated to the right. The block pattern is fully curvilinear, following a traditionally “suburban” pattern. Some developments are only accessible via private streets, and large-scale private development begins to form barriers to connectivity. In this example, Residential and non-residential areas are moderately connected, with a distinct trend of culs-de-sac off of a more connected street.



Typical Open Space Typologies

Suburb neighborhoods have a variety of park and open space types. Park typologies found near the Ridge neighborhood range from activated School Parks serving the immediate neighborhood, to non-activated open spaces.



Eagleridge Open Space
Park Type: Open Space
Size: 3.9 acres



Northridge School Park
Park Type: School Park
Size: 7.7 acres

Typical Street Typologies

Streets in the Ridge neighborhood represent several functional classes, typically ranging from arterials to local streets. The streetscape amenity zone is rarely found in this pattern, and sidewalks often lack protection from nearby traffic. The majority

of right-of-way space is dedicated to vehicular use through travel lanes and space for on-street parking.

Note: Roadway widths are measured from curb edge to curb edge.



Sidewalk	Bike Lane	Buffer	Travel Lane	Turn Lane	Travel Lane	Buffer	Bike Lane	Sidewalk
4.5'	8'	4'	11.5'	12'	12'	4'	8'	4.5'
Roadway 60'								

Outlook Boulevard
Functional Class: Minor Arterial
Roadway Width: 60'
Multimodal Feature(s): Separated Bike Lane, Buffered Sidewalk



Sidewalk	Parking Lane	Travel Lane	Travel Lane	Parking Lane	Sidewalk
4.5'	7.5'	9'	9'	7.5'	4.5'
Roadway 33'					

Aquila Drive
Functional Class: Local
Roadway Width: 33'
Multimodal Feature(s): N/A



Sidewalk	Parking Lane	Travel Lane	Travel Lane	Parking Lane	Sidewalk
5'	7.5'	7.5'	7.5'	7.5'	5'
Roadway 33'					

Pso Del Norte
Functional Class: Local
Roadway Width: 33'
Multimodal Feature(s): N/A

Street Design Elements

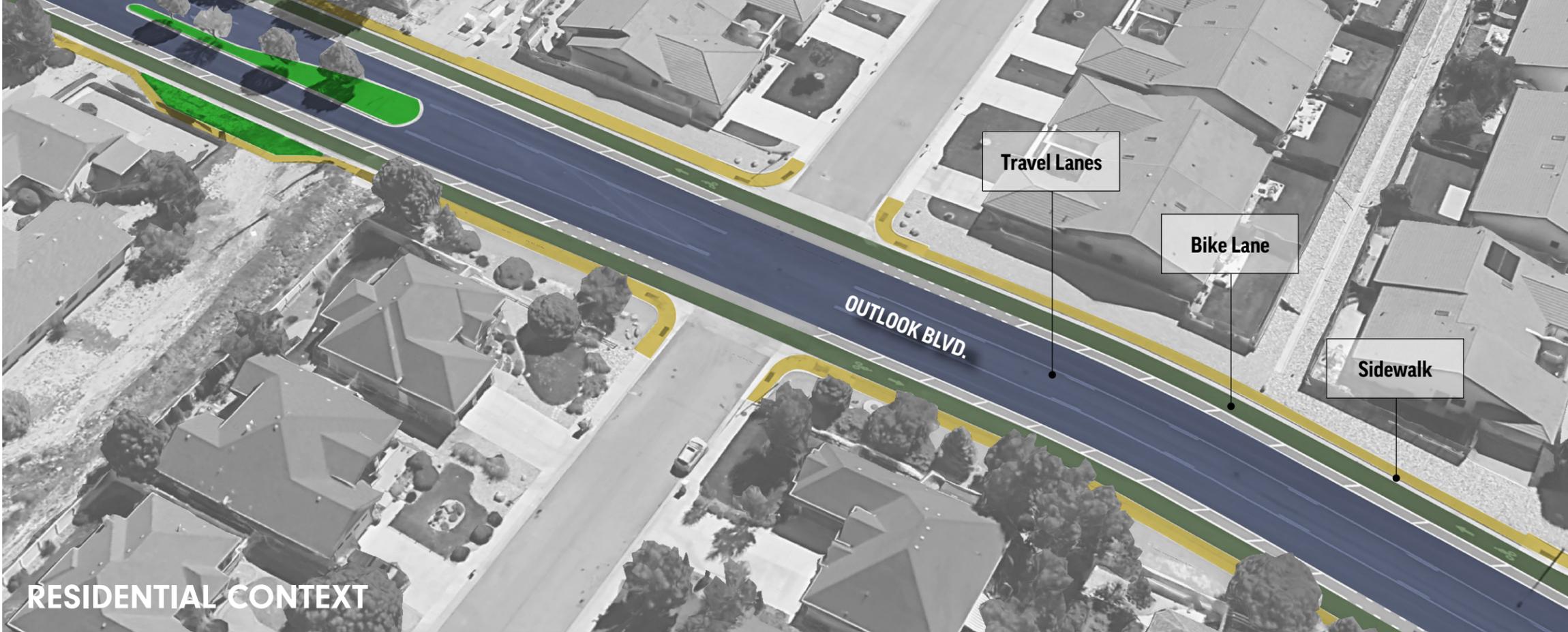
Street Design Elements

-  Sidewalk
-  Bike Lane
-  Streetscape Amenity Zone
-  Parking
-  Travel Lane

Context	Residential	Non-residential
Roadway Width	60'	45'
Roadway	Buffered Bike Lanes	Bike Lanes
Streetscape Elements	N/A	N/A

Street Design & Transition

Outlook Boulevard is an example of a street in Suburb pattern areas that transitions from a non-residential context to a residential context as it weaves through the Ridge neighborhood. As the context around the street changes, the design of the street also changes. In this residential context, Outlook Boulevard includes bike lanes, and protected sidewalks. As Outlook Boulevard moves into non-residential areas, the bike lanes remain but are no longer protected.



Typical Urban Design Elements

Urban Design Elements

- █ Front Building Line
- █ Sidewalk
- █ Access (Driveways & Parking)
- █ Streetscape Amenity Zone

In this example area within the Ridge neighborhood, the pattern of urban design elements remains consistent between the different housing types. There is a somewhat consistent sidewalk network (with a few areas of no or limited sidewalk), although it is frequently disrupted by private driveways, and does not include a streetscape amenity zone. The consistency of the front building line establishes a similar frontage pattern that assists the detached homes and duplexes to integrate well together.



Example Block Location



Typical Urban Design Elements

This series of diagrams takes a closer look at a portion of the example block (shown in plan view on the previous page).

Front Building Line
The example block has a consistent pattern of front building lines.

Access (Driveway & Parking)
Curb cuts for private driveways interrupt the sidewalk.

Sidewalk
The example block has consistent sidewalk presence, but there are frequent breaks (indicated by the dashed lines) in the sidewalk for private driveways.



Frontage Types

Entry are typically recessed, and may include a small porch or internal terrace. Driveways are typically the dominant feature of frontage in this area.

Frontage & Access

 Entry Features

 Access (Driveways & Parking)



Entry Features: Stoop & Porch | Access Format: Front Driveway
Parking Format: Front Attached Garage



Entry Features: Recessed Stoop | Access Format: Front Driveway
Parking Format: Front Attached Garage



Entry Features: Recessed Stoop | Access Format: Side Driveway
Parking Format: Side Attached Garage



Entry Features: Gated Terrace & Stoop | Access Format: Front Driveway
Parking Format: Front Attached Garage

Building Types

This neighborhood includes a variety of housing types, though they are generally clustered together and not integrated throughout the blocks. Types include detached homes, duplexes, townhomes, and a unique arrangement of courtyard apartments. Scales range from medium to large.



Duplex

Scale: Medium | Building Footprint: approx. 5,480 ft²
Lot Size: approx. 11,210 ft² | Height: 1 stories | Frontage Type: Suburban Yard



Townhomes

Scale: Medium | Building Footprint: approx. 10,417 ft²
Lot Size: approx. 27,910 ft² | Height: 1 story | Frontage Type: Suburban Yard



Courtyard Apartments

Scale: Large | Building Footprint: approx. 10,930 ft²
Lot Size: approx. 26,760 ft² | Height: 2 stories | Frontage Type: Buffer



Detached House

Scale: Medium | Building Footprint: approx. 2,920 ft²
Lot Size: approx. 7,400 ft² | Height: 1 story | Frontage Type: Suburban Yard



Non-Residential Context 01.

Ridge & North Side Neighborhoods

Commercial District

Development Pattern Example Area

This commercial area, located where the Ridge neighborhood and the North Side neighborhood border each other, is a portion of a larger commercial corridor running along both sides of US-50. It is a typical example of a non-residential context within the Suburb development pattern.



Block Pattern

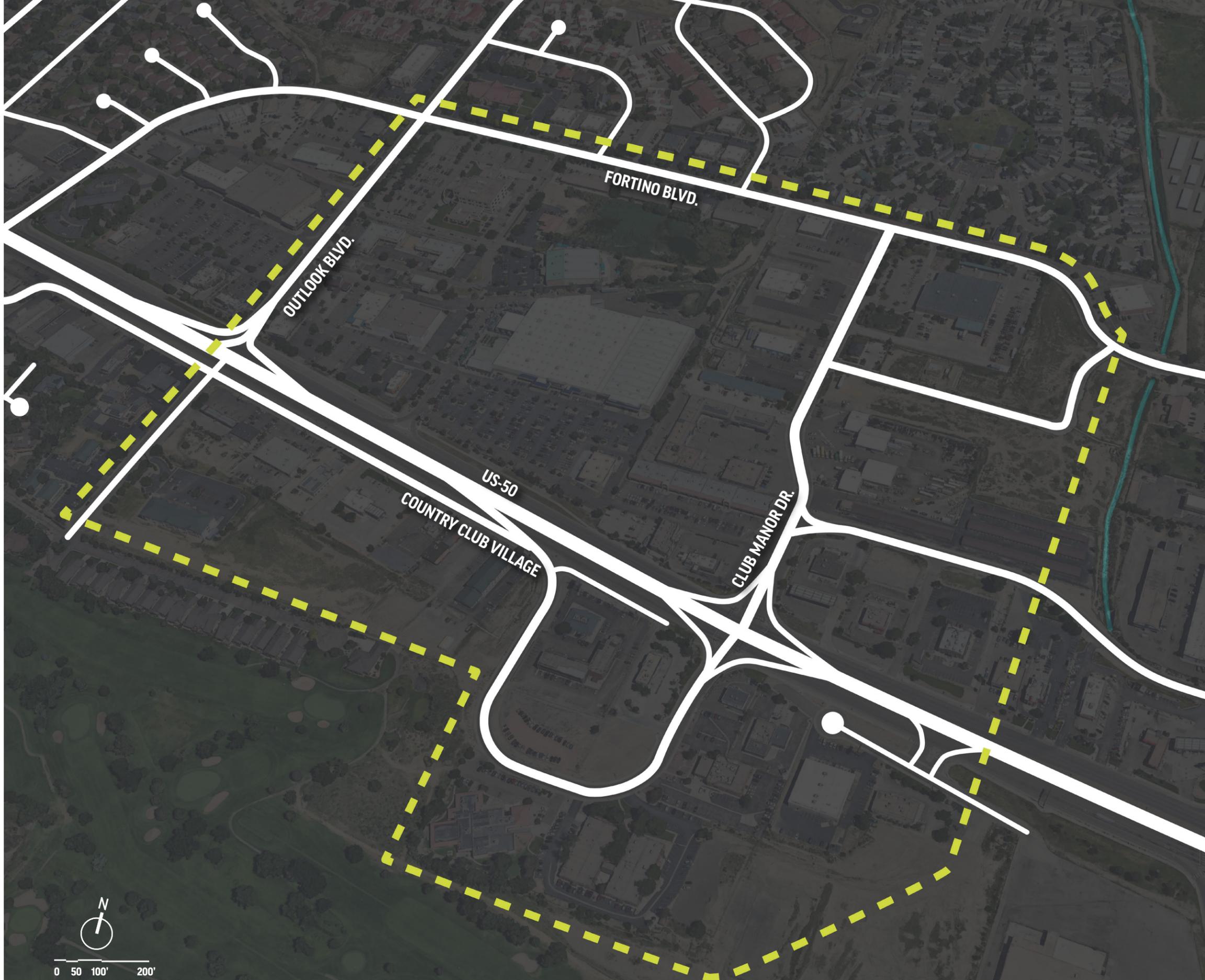
Suburb Block Pattern

Block Size: varies, approx. 5 acres - 52 acres

Block Dimensions: varies, approx. 510' x 420' - 1,660' x 1,400'

- Street Network
- - - Example Boundary

Non-residential Suburb pattern blocks vary widely in size, ranging from five acres to fifty-two acres in the example area illustrated to the right. A pattern of large-scale commercial developments with frontages dominated by surface parking continues in this pattern, often with an outer layer of pad sites ringing an inner layer of very large commercial sites.



Typical Open Space Typologies

Suburb areas have a variety of park and open space types. Park typologies accessible from the Ridge and North Side non-residential areas include activated and non-activated open space types.



Eagleridge Open Space
Park Type: Open Space
Size: 3.9 acres



Northridge School Park
Park Type: School Park
Size: 7.7 acres



Fountain Creek Trail
Park Type: Open Space & Trail
Size: 430 acres

Typical Street Typologies

Street types in non-residential Suburb areas fall into a range of functional classes. The streetscape amenity zone is rarely present, and while sidewalks are consistently present, they have no buffering from the roadway and are typically very narrow. Bike lanes are occasionally present but are not protected.

Overall, these street typologies favor automobile transportation over all other modes.

Note: Roadway widths are measured from curb edge to curb edge.



Sidewalk	Bike Lane	Travel Lane	Center Turn Lane	Travel Lane	Bike Lane	Sidewalk
6.5'	5.5'	11.5'	13'	11.5'	5.5'	6.5'
Roadway						
						47'

Outlook Boulevard
Functional Class: Minor Arterial
Roadway Width: 47'
Multimodal Feature(s): Bike Lane, Buffered Sidewalk



Sidewalk	Bike Lane	Travel Lane	Center Turn Lane	Travel Lane	Bike Lane	Sidewalk
6.5'	5.5'	11'	10.5'	10.5'	5.5'	7'
Roadway						
						43'

Fortino Boulevard
Functional Class: Minor Arterial
Roadway Width: 43'
Multimodal Feature(s): Bike Lane, Buffered Sidewalk



Sidewalk	Travel Lane	Travel Lane	Buffer
6.5'	18'	18'	2'
Roadway			
			36'

Country Club Village
Functional Class: Collector
Roadway Width: 36'
Multimodal Feature(s): N/A

Street Design Elements

Street Design Elements

- Sidewalk
- Bike Lane
- Streetscape Amenity Zone
- Parking
- Travel Lane

Context	Residential A	Residential B	Non-residential
Roadway Width	55'	60'	45'
Roadway	N/A	Buffered Bike Lanes	Bike Lanes
Streetscape Elements	Streetscape amenity zone on one side of street; buffered sidewalk.	N/A	N/A

Street Design & Transition

Outlook Boulevard is a street within the Suburb pattern that transitions between a non-residential context and two different residential contexts (documented earlier in this section). In the non-residential context, Outlook Boulevard includes (unprotected) bike lanes, narrow sidewalks, and dedicates the majority of space to vehicular transportation. In the residential contexts, Outlook Boulevard includes buffered sidewalks (buffered by either a streetscape amenity zone or adjacent protected bike lanes).



Typical Urban Design Elements

Urban Design Elements

- Front Building Line
- Sidewalk
- Access (Driveways & Parking)
- Streetscape Amenity Zone
- Curb Cut

In Suburb non-residential areas, frontage patterns are inconsistent. Front building lines are sometimes oriented towards the street, but are often oriented internally to the site. Large quantities of surface parking dominates frontages, and while the sidewalk is continuous it is frequently interrupted by curb cuts. The streetscape amenity zone is rare and lacks any amenities.



Example Block Location

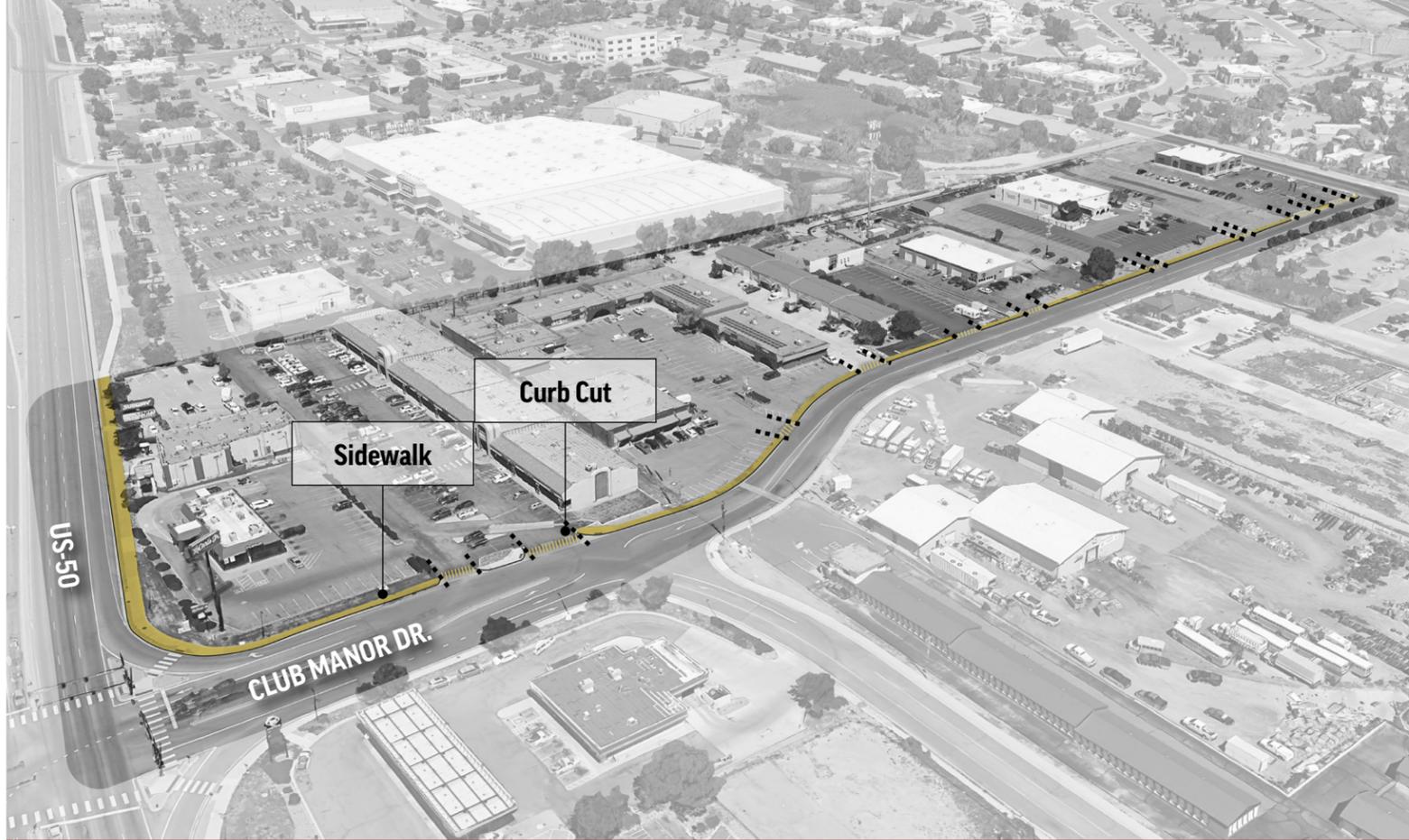


Typical Urban Design Elements

A series of diagrams shows a closer look at a portion of the example block shown in plan view on the previous page.

-  Sidewalk
-  Curb Cut

The example block has consistent sidewalk presence, but there are frequent breaks (indicated by the dashed lines) for curb cuts.



-  Access (Driveway & Parking)
-  Curb Cut

Access and parking is the dominant feature of Suburb sites. Large surface parking lots dominate the space between buildings and the street.



Typical Urban Design Elements

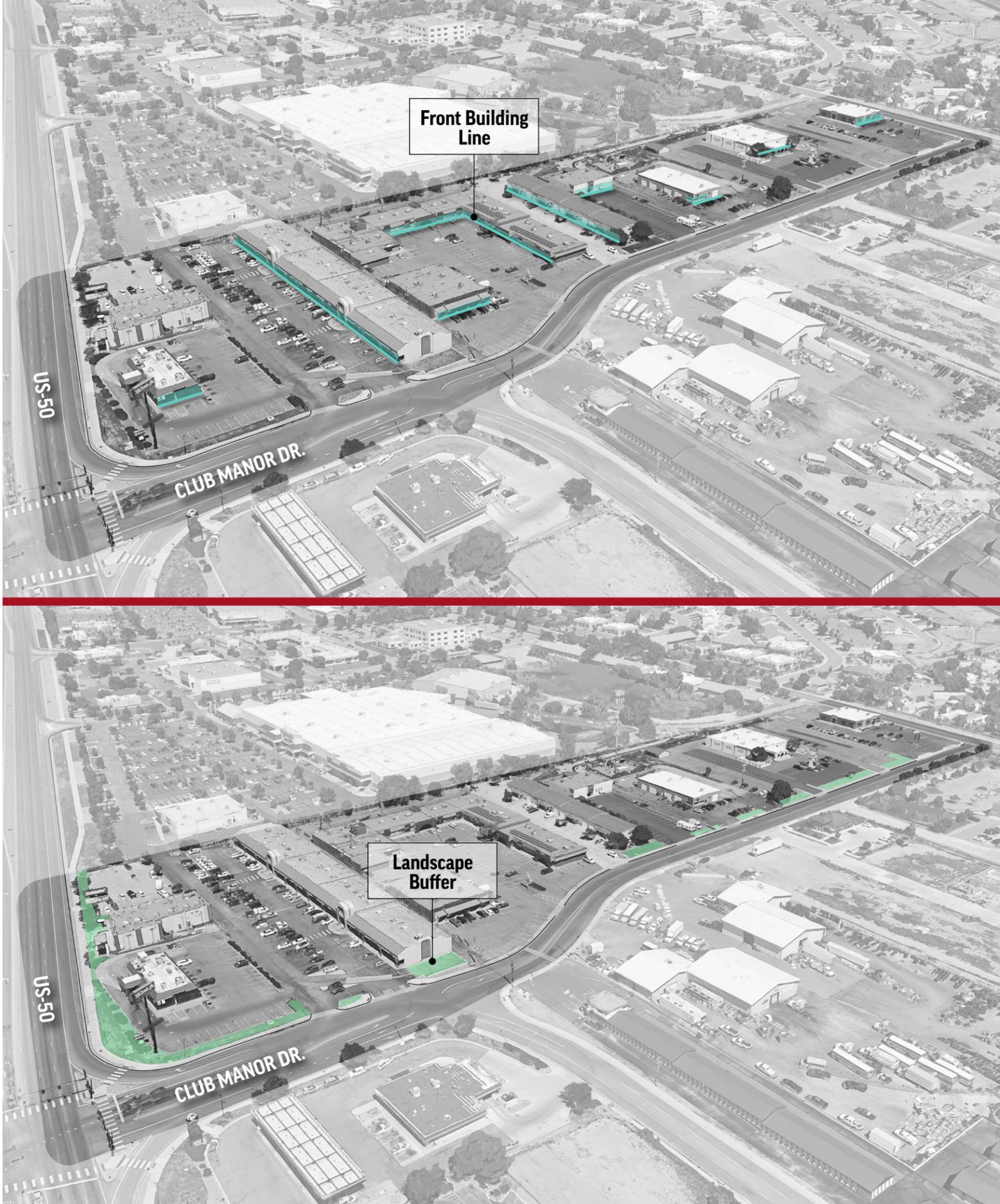
A series of diagrams shows a closer look at a portion of the example block shown in plan view on the previous page.

- Ground Floor
- Front Building Line

Large setbacks and inconsistent front building line patterns create little relationship between the street and private development.

- Buffer Space

The blocks lack a streetscape amenity zone, and instead turf landscape buffers serve to separate private development from the street.



Building Types

Non-residential Suburb areas include a variety of building types oriented towards providing a regional commercial center. Retail of all scales, drive-thrus, gas stations, medical facilities, and institutional services like a post office are common. Larger-scale commercial buildings are typically located towards the rear or centers of sites, with smaller-scale pad sites or drive-thrus located towards the front of sites.



Big Box Store

Scale: Large | Building Footprint: approx. 188,195 ft² | Land Use: Commercial
Lot Size: approx. 13 acres | Height: 1 story | Frontage Type: Parking & Buffer



Commercial Pad

Scale: Small | Building Footprint: approx. 7,760 ft² | Land Use: Commercial
Lot Size: approx. 37,450 ft² | Height: 1 story | Frontage Type: Parking



Medical Clinic

Scale: Large | Building Footprint: approx. 30,392 ft² | Land Use: Commercial
Lot Size: approx. 242,317 ft² | Height: 1-3 stories | Frontage Type: Buffer & Parking



Office Building (Medical)

Scale: Small | Building Footprint: approx. 5,028 ft² | Land Use: Commercial
Lot Size: approx. 57,775 ft² | Height: 1 story | Frontage Type: Parking & Buffer



Drive-Thru

Scale: Small | Building Footprint: approx. 2,500 ft² | Land Use: Commercial
Lot Size: approx. 123,678 ft² | Height: 1 story | Frontage Type: Buffer



Commercial Strip (Retail)

Scale: Medium | Building Footprint: approx. 12,156 ft² | Land Use: Commercial
Lot Size: approx. 123,678 ft² | Height: 1 story | Frontage Type: Parking & Buffer



Commercial Strip (Offices)

Scale: Medium | Building Footprint: 10,127 ft² | Land Use: Commercial
Lot Size: 31,328 ft² | Height: 1 stories | Frontage Type: Buffer & Parking



Post Office

Scale: Medium | Building Footprint: approx. 41,570 ft² | Land Use: Institutional
Lot Size: 222,321 ft² | Height: 1 story | Frontage Type: Buffer & Parking

Key Takeaways

Block Pattern

- The Suburb block pattern becomes fully irregular, curvilinear, and often characterized by culs-de-sac. Blocks in a small area may vary greatly in size, negatively impacting connectivity.



Open Space

- Open space types are inconsistent. Some neighborhoods may include a variety of open space types, and others may include none.



Streets

- The majority of street space is dedicated to vehicular transportation. Bike lanes may be present but are often unprotected. Sidewalks are inconsistent and often very narrow with little protection from the roadway.



Urban Design Elements & Frontage

- Parking and access dominate the frontage in this pattern. Residential areas prominently feature wide driveways and front-loaded garages. Non-residential areas feature very frequent curb cuts for front parking lot access. Non-residential buildings often do not face the street.



Building Types

- In residential areas, building types may include detached homes, duplexes, and apartment buildings, but these are typically found in separate developments. Non-residential buildings range from very small to very large in scale, and are typically clustered into commercial districts or centers.



Ridge Neighborhood, Pueblo



4 EDGE

Introduction

The Edge development pattern is typically found on the edges of Pueblo, bordering or very close to the city limits. This pattern may resemble suburban or exurban developments, but is often surrounded by undeveloped land, forming an “island” of development at the outer edges of the city. The development pattern only includes residential areas, so a non-residential context is not documented in this section.

Block sizes are moderate to large and alleys are not incorporated. Residential streets typically have a continuous sidewalk network, but do not include bike lanes. A streetscape amenity zone buffers sidewalks users from the roadway, but

there frequent breaks in the sidewalk and the streetscape amenity zone for very wide private driveways. Frontage features come in a wide variety: some homes feature side driveways, side or rear parking, and porches that address the street, but others are dominated by driveways and garages with barely-visible recessed entrances.

Varying house by house, the overall relationship between the public realm and private development is inconsistent in this pattern - incorporating some elements from each of the previously documented patterns (Grid, Transition, and Suburb).



Residential Context 01.

Regency Neighborhood

Edge Neighborhood

--- Development Pattern Example Area

This area within the Regency neighborhood is a typical example of the Edge development pattern in Pueblo.



Block Pattern

Edge Block Pattern

Block Size: varies, approx. 8 acres - 20 acres

Block Dimensions: varies, approx. 400' x 920' - 540' x 1,500'

- Street Network
- Trails
- - - Example Boundary
- Parks & Open Space

Blocks in the Edge development pattern range in size, from eight acres to twenty acres in the example area illustrated to the right. The pattern is curvilinear, and the defining feature is a series of internal culs-de-sac jutting out from a central spine. Connectivity between the neighborhood and destinations outside of the neighborhood is limited by the neighborhood's location on the outer edge of Pueblo city limits and the internalized nature of the development.



Typical Open Space Typologies

The open space features within the neighborhood are two “central greens:” one in the center of a roundabout, and the other buffering overflow parking. For access to parks and other activated open spaces, the neighborhood relies on parks located within nearby neighborhoods.



Central Green
Park Type: Landscaped Green
Size: 0.65 acres

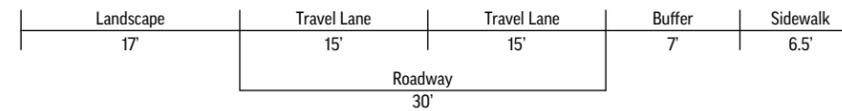


Central Green (2)
Park Type: Landscaped Green
Size: 0.52 acres

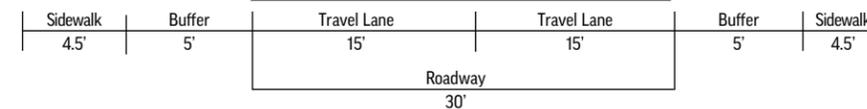
Typical Street Typologies

Streets within the neighborhood are local streets, while streets that provide access through and out of the neighborhood are typically local or minor arterial functional classes. The streetscape amenity zone is found in this pattern, and protects pedestrians from the roadway. Travel lane widths are extremely wide.

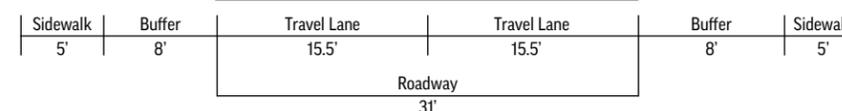
Note: Roadway widths are measured from curb edge to curb edge.



Bandera Parkway
Functional Class: Minor Arterial
Roadway Width: 30'
Multimodal Feature(s): Buffered Wide Sidewalk



Ventana Circle
Functional Class: Local
Roadway Width: 30'
Multimodal Feature(s): Buffered Sidewalks



Terracina Place
Functional Class: Local
Roadway Width: 31'
Multimodal Feature(s): Buffered Sidewalks

Typical Urban Design Elements

Urban Design Elements

- Front Building Line
- Sidewalk
- Access (Driveways & Parking)
- Streetscape Amenity Zone

Front building lines form a relatively consistent pattern throughout this neighborhood. The sidewalk is continuous within the neighborhood, but less consistent immediately outside of the development. Private driveways cause frequent interruptions to the sidewalk. The streetscape amenity zone is continuous with the sidewalk, and in some areas provides shade trees for pedestrians.



Example Block Location

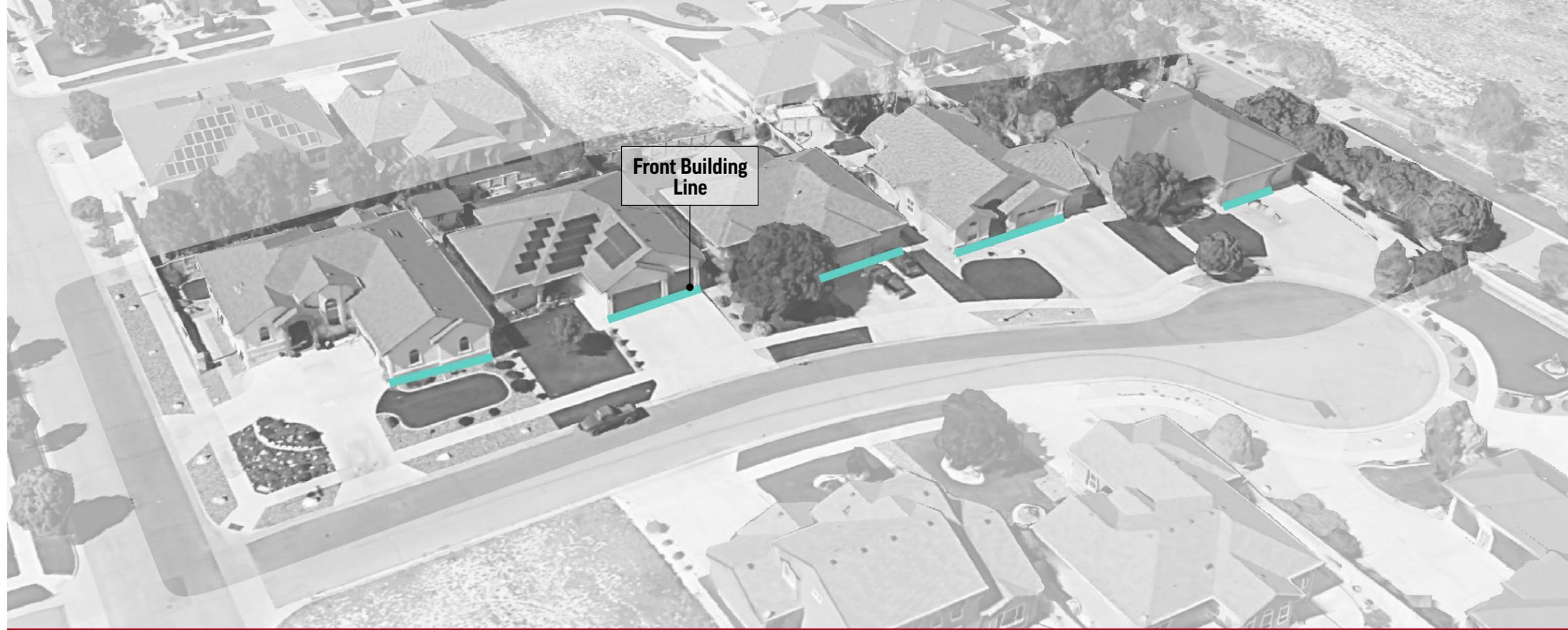


Typical Urban Design Elements

This series of diagrams takes a closer look at a portion of the example block (shown in plan view on the previous page).

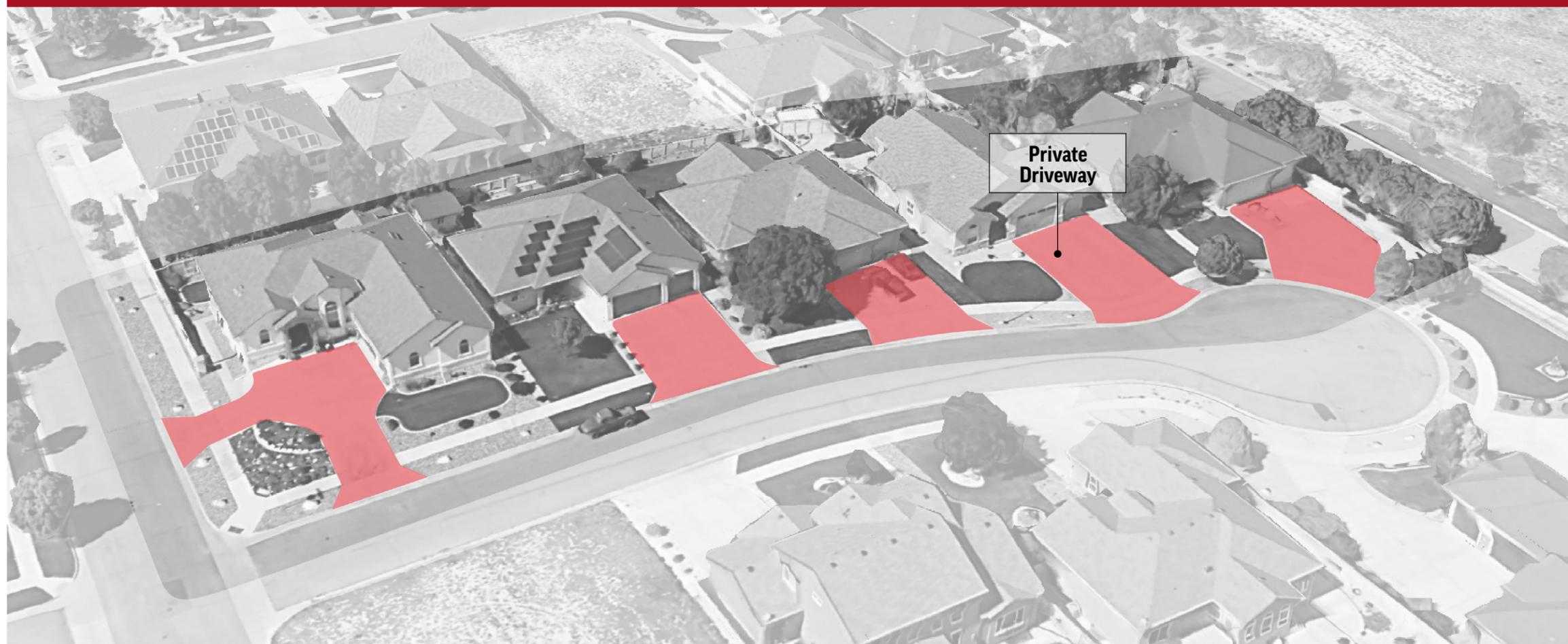
Front Building Line

The example block has a somewhat consistent pattern of front building lines and setbacks.



Access (Driveway & Parking)

The block's pattern of front driveways and front-loaded garages cause wide, frequent interruptions to the sidewalk.



Typical Urban Design Elements

A series of diagrams shows a closer look at a portion of the example block shown in plan view on the previous page.

Streetscape Amenity Zone

The streetscape amenity zone includes a landscaped strip, buffering pedestrians from the roadway. There are breaks for private driveways.

Sidewalk

The sidewalk is continuous, only interrupted by wide private driveways.



Frontage Types

Frontage types vary in this neighborhood. Stoop, porch, and recessed entries are all common. Many homes feature very wide front driveways and front-loaded garages, but some feature side driveways with parking located to the side or rear of the lot.

Frontage & Access

- Entry Features
- Access (Driveways & Parking)



Entry Features: Recessed Stoop | Access Format: Side Driveway
Parking Format: Side Attached Garage



Entry Features: Porch & Recessed Stoop | Access Format: Front Driveway
Parking Format: Side Attached Garage



Entry Features: Recessed Stoop & Porch | Access Format: Front Driveway
Parking Format: Front Attached Garage



Entry Features: Recessed & Stoop | Access Format: Side Driveway
Parking Format: Side Attached Garage



Entry Features: Recessed Stoop | Access Format: Front Driveways
Parking Format: Front Attached Garage



Entry Features: Recessed | Access Format: Front Driveway
Parking Format: Front Attached Garage

Building Types

Building types range in scale from medium to large, with the one exception being an example of an Accessory Dwelling Unit (ADU). The neighborhood primarily includes detached homes, with a few duplexes.



Detached House

Scale: Large | Building Footprint: approx. 5,630 ft²
 Lot Size: approx. 14,715 ft² | Height: 1.5 stories | Frontage Type: Neighborhood Yard



Detached House

Scale: Medium | Building Footprint: approx. 2,250 ft²
 Lot Size: approx. 11,180 ft² | Height: 2 stories | Frontage Type: Neighborhood Yard



Duplex

Scale: Medium | Building Footprint: approx. 5,710 ft²
 Lot Size: approx. 18,860 ft² | Height: 1 story | Frontage Type: Suburban Yard



Detached House

Scale: Large | Building Footprint: approx. 6,230 ft²
 Lot Size: approx. 25,125 ft² | Height: 1 story | Frontage Type: Suburban Yard



Duplex

Scale: Medium | Building Footprint: approx. 5,590 ft²
 Lot Size: approx. 14,630 ft² | Height: 1.5 stories | Frontage Type: Suburban Yard



Accessory Dwelling Unit (ADU)

Scale: Small | Building Footprint: 1,328 ft²
 Lot Size: approx. 18,050 ft² | Height: 2 stories | Frontage Type: Suburban Yard

Key Takeaways

Block Pattern

- The Edge block pattern is curvilinear and characterized by many culs-de-sac. Block sizes are typically moderate to large. Connectivity is often poor due to limited access in and out of Edge developments.



Open Space

- Open space typologies are limited and primarily include non-activated landscaped central greens.



Streets

- Streets are very wide and oriented towards automobile usage. There is room for on-street parking but may be rarely utilized. Sidewalks are moderately wide and often protected from the roadway by a streetscape amenity zone, but may not connect to destinations outside of the development.



Urban Design Elements & Frontage

- Private driveways and large, front-loaded garages are prominent frontage features. Porches may be present, while entries are often recessed. Front buildings form a consistent pattern oriented towards the street.



Building Types

- Building types are primarily detached homes. Some duplexes and ADUs are present. The scale of buildings ranges from medium to large.



Regency Neighborhood, Pueblo



city of

PUEBLO

colorado